

Old Mangere Bridge Replacement Engagement Report

August 2014



Old Mangere Bridge Replacement

Engagement Report

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1 Introduction

This report presents the responses and an analysis of community engagement undertaken as part of the New Zealand Transport Agency's (the Transport Agency) Old Mangere Bridge Replacement Project.

In 2012, the Transport Agency and project team sought community views on the current use, features and issues associated with the existing bridge as well as seeking to identify key features desired in the replacement bridge. Feedback was analysed and presented in a report – *Old Mangere Bridge Replacement Community Engagement Report October 2012*. The feedback received was considered and incorporated where appropriate into the development of a draft design concept.

A draft design concept, incorporating 2012 feedback, was presented to the community as part of the 2014 engagement and general feedback sought (no specific questions were asked).

This report outlines the engagement approach taken and the feedback received from the community during the engagement period of July/August 2014. It also incorporates a summary of engagement undertaken during 2012 in order to provide a complete overview of engagement undertaken and key feedback themes for the project overall.

1.1 Post Script

Since 2014, engagement has focused on working with Mana Whenua to incorporate shared heritage, stories and values into the design of the bridge.

2 Project background

The Old Mangere Bridge is a well utilised community facility, providing access across the Manukau Harbour for pedestrians and cyclists between Onehunga and Mangere Bridge townships as well as being a popular recreation and fishing spot.

Currently, the Old Mangere Bridge requires expensive maintenance to hold it at its current level of service. The structural condition of the bridge is rapidly deteriorating and is not expected to be able to provide a safe access for more than another three to five years. There is also a legal requirement for the Agency to maintain a cross harbour pedestrian/cycle link in this location.

Because of its popularity and strategic location for pedestrians and cyclists, the Transport Agency intend to replace the existing bridge with a new bridge suitable to the activities currently undertaken. There is strong community attachment to the existing bridge and community interest surrounding its replacement is very high.

2.1 Bridge history

The Old Mangere Bridge was constructed in 1914 and is believed to be the oldest reinforced concrete bridge crossing a harbour in New Zealand. It replaced an earlier wooden bridge that existed in a similar location. Until 1983, what is now known as the Old Mangere Bridge provided the primary vehicular access across the Manukau Harbour.

In 1983, the SH20 Mangere Bridge opened to the east of the Old Mangere Bridge. With the opening of the new SH20 crossing, the Old Mangere Bridge was closed to all traffic except pedestrians and cyclists.

The Old Mangere Bridge is now a very popular community facility, used by pedestrians and cyclists as well as being a popular fishing spot. The bridge provides the most direct route between the Onehunga and Mangere Bridge townships for pedestrians and cyclists. The bridge forms a vital part of the Regional Walking and Cycling Network, linking the recently completed SH20 Mt Roskill Cycleway and Waikaraka Cycleway at the northern edge of the Manukau Harbour, to Mangere Bridge Township and further south towards the Auckland International Airport.

2.2 Project objectives

The objective of the Old Mangere Bridge Replacement Project is:

To ensure a safe connection for walking and cycling is provided across Manukau Harbour in the general vicinity of the existing Old Mangere Bridge.

This objective is derived from condition 24 of the SH20 Manukau Harbour Crossing designation¹.

¹ SH20 Manukau Harbour Crossing (ref 182A within the Manukau Section of the District Plan). Condition 24 of this designation requires that:

“The Requiring Authority shall ensure that provision for continued public walking and cycling passage across Manukau Harbour generally between Coronation Road, Mangere Bridge and Onehunga Harbour Road, Onehunga, in the general vicinity of the Old Mangere Bridge, is kept and maintained.”

In addition to this primary objective, the Transport Agency is committed to retaining existing activities on the replacement bridge (such as fishing) where these activities do not unduly compromise the primary project objective.

2.3 Project timeframes

The indicative timeframes for the project (as at August 2014) are as follows:

July – August 2014 – Community engagement

April – October 2014 – Design with artist involvement

November 2014 – Seek consents from Council

2015 – Construction starts

2016 – Estimated construction completion.

3 Summary of previous engagement

There is strong community attachment to the bridge and it is highly valued for its access, recreational and heritage values. This has been demonstrated throughout a number of projects in the area in recent years including the Manukau Harbour Crossing Project and the replacement boat ramp works. These values and sentiments were further demonstrated throughout the initial public engagement period for this project, held from July to September 2012.

A range of engagement and feedback mechanisms were employed during this period:

- A Project Reference Group was established to act as a governance group for the project to help provide direction on decision-making.
- A hui was held at Te Puea Marae, Mangere Bridge. Iwi holding Mana Whenua or Kaitiaki were then invited to be a part of the Project Reference Group.
- Workshops were held with the Mangere-Otahuhu Local Board and the Maungakiekie-Tamaki Local Board. Subsequent to these, both Local Boards appointed a representative to be on the Project Reference Group.
- Letters were sent to all other stakeholders, including schools, business associations and community groups.
- Local schools were invited to participate in a unique process designed to facilitate feedback from children in the community. Three schools took part – Waterlea Primary School, Mangere Bridge Primary School and Onehunga Primary School.
- Two community information open days were held and were very well attended – with over 800 estimated to have attended over the two days. The format of the two information open days in 2012 was very similar to that of the 2014 information open day – as described in Section 5.

Overall, more than 800 people were estimated to have participated in the engagement and 705 written submissions were received.

Key themes from the feedback received can be summarised as:

- A desire to have the replacement bridge as wide as possible – similar to the current bridge width of 12 metres.
- Improved safety features need to be incorporated, including hand-railings and lighting.
- The ability to continue to undertake all current activities (including fishing, walking, cycling) in a safe and comfortable manner on the replacement bridge.
- Reflect (or preserve where possible) the history and character of the old bridge in the replacement bridge.

Further detail on the outcomes of previous engagement can be found in the *Old Mangere Bridge Replacement Community Engagement Report* dated October 2012.

4 Engagement overview

This section of this report summarises the engagement planning process, including the objectives of engagement, the framework upon which the 2014 engagement was undertaken, the methodology and the parties consulted. Further detail is set out in the *Public Engagement Plan* dated June 2014 and attached as Appendix 1.

4.1 Engagement framework

The communication and engagement process is aligned with the principles and core values of the International Association of Public Participation (IAP2) as well as the consultation requirements of the Land Transport Act 2003 and Local Government Act 2002.

The IAP2 provides internationally recognised consultation best practice principles. It sets out a community engagement spectrum of participation depending on the decisions to be made and the associated level of influence (if any) the community can make to a project.

For this engagement period, the community were engaged at the *Inform - Consult* level. The different levels of the parties consulted are outlined in Section 4.3 below.

4.2 Engagement objectives

The **goal** of the Public Engagement for Old Mangere Bridge is to:

Provide key stakeholders and the community with the opportunity to be informed about the project and for them to be able to provide feedback to the team about specific project design elements.

The **promise** that is being made through the Public Engagement Plan is:

The Transport Agency will keep stakeholders and the community informed throughout the project, listen to and acknowledge concerns and aspirations, and provide feedback on how stakeholder input influenced our decisions.

The **objectives** of the engagement are to:

- Listen to and acknowledge all feedback received and provide clarity as to how feedback has been taken into account in the development of the project.
- Ensure communication materials are easy to understand, timely and accessible.
- Allow enough time for stakeholders and the community to contribute meaningfully.
- Ensure that the principles and core values of the International Association of Public Participation (IAP2) are met throughout all stages of consultation.
- Meet the requirements of the Land Transport Act 2003 and Local Government Act 2002 as well as the requirements of the Resource Management Act 1991 in relation to consents which will be applied for.

4.3 Parties consulted and methods utilised

4.3.1 Project Reference Group

A Project Reference Group was established in April 2012 to act as a governance group for the project to help provide direction on decision making. The parties mainly represent those who have a statutory interest in the Old Mangere Bridge. The Project Reference Group consists of representatives from:

- NZTA
- Auckland Council
- Auckland Transport
- Historic Places Trust
- Local Boards (June 2012 onwards)
- Iwi (July 2012 onwards)

The Auckland Harbourmaster and Port of Onehunga are non-attending Project Reference Group members, with all minutes of meetings sent to them for information.

The Project Reference Group is chaired by NZTA and meets regularly to discuss and progress various elements of the Old Mangere Bridge Replacement Project.

The Project Reference Group were provided a brief prior to the community open day and received advanced copies of the project newsletter via email. A number of members also volunteered to attend the open day and participate in community discussions.

The Project Reference Group have participated in the project at the *consult-involve* level on the IAP2 spectrum.

4.3.2 Iwi

During 2014, a Mana Whenua Forum/ Roopu was established and a number of hui held. The Mana Whenua representative include:

- Ngati Whatua o Orakei
- Ngati Te Ata
- Ngati Tamaoho
- Te Aki Tai
- Kawerau A Maki
- Makarau Mare

- Te Puea Memorial Marae

The purpose of the forum was to share agreed stories, heritage and values that could be reflected in the design of the new bridge.

In the lead up to the community open day, all iwi representatives were contacted via email by the Transport Agency and invited to attend the open day. A copy of the project newsletter was attached to the email sent.

In addition to the Roopu, Iwi were invited to be part of the Project Reference Group.

Iwi have participated in the project at the *consult-involve* level on the IAP2 spectrum.

4.3.2.1 Post Script

Since 2012 Mana Whenua have had an active role in working with the design team so that shared values can be reflected in the design. Six hui have been held during 2015.

In 2015 NZTA with the input of Mana Whenua commissioned two artists to gather the collective stories, heritage and cultural values and work with the design team to incorporate these into the bridge design. This process is still being worked through with some very positive design outcomes emerging.

4.3.3 Stakeholders

Other stakeholders (not including those on the Project Reference Group) with an identified interest in the project include local schools, business associations, and interest and action groups. A full list of stakeholders contacted is attached as Appendix 2.

A letter was sent to all stakeholders on 11 July 2014 providing them with a project update and inviting them to contact the project team or attend the community open day. A copy of the newsletter accompanied the letters. A copy of the newsletter is attached as Appendix 3.

Stakeholders have participated in the project at the *inform - consult* level on the IAP2 spectrum.

4.3.4 Wider community

The engagement period for the wider community began in July 2014 with a media release issued on 11 July 2014. A copy of the media release is attached as Appendix 4.

Newsletters were delivered the Tuesday 14th and Wednesday 15th of July. A copy of the newsletter is attached as Appendix 3. Approximately 20,000 newsletters were delivered to the local community – to the area shaded on the map in Figure 1 overleaf.

An information open day was planned for and held on 26 July 2014 at Waterfront Road Reserve, Mangere Bridge. The information day is outlined in further detail in Section 5 below. Advertising posters were placed on noticeboards in the Mangere, Mangere Bridge and Onehunga areas as well as being placed at the Mangere Bridge end of the bridge itself. Facebook and the Transport Agency website were also used to announce the open day.

In addition, the NZTA website contained information about the project and details of how to provide feedback and contact the project team.

The community have participated in the project at the *inform* level on the IAP2 spectrum during this round of engagement.

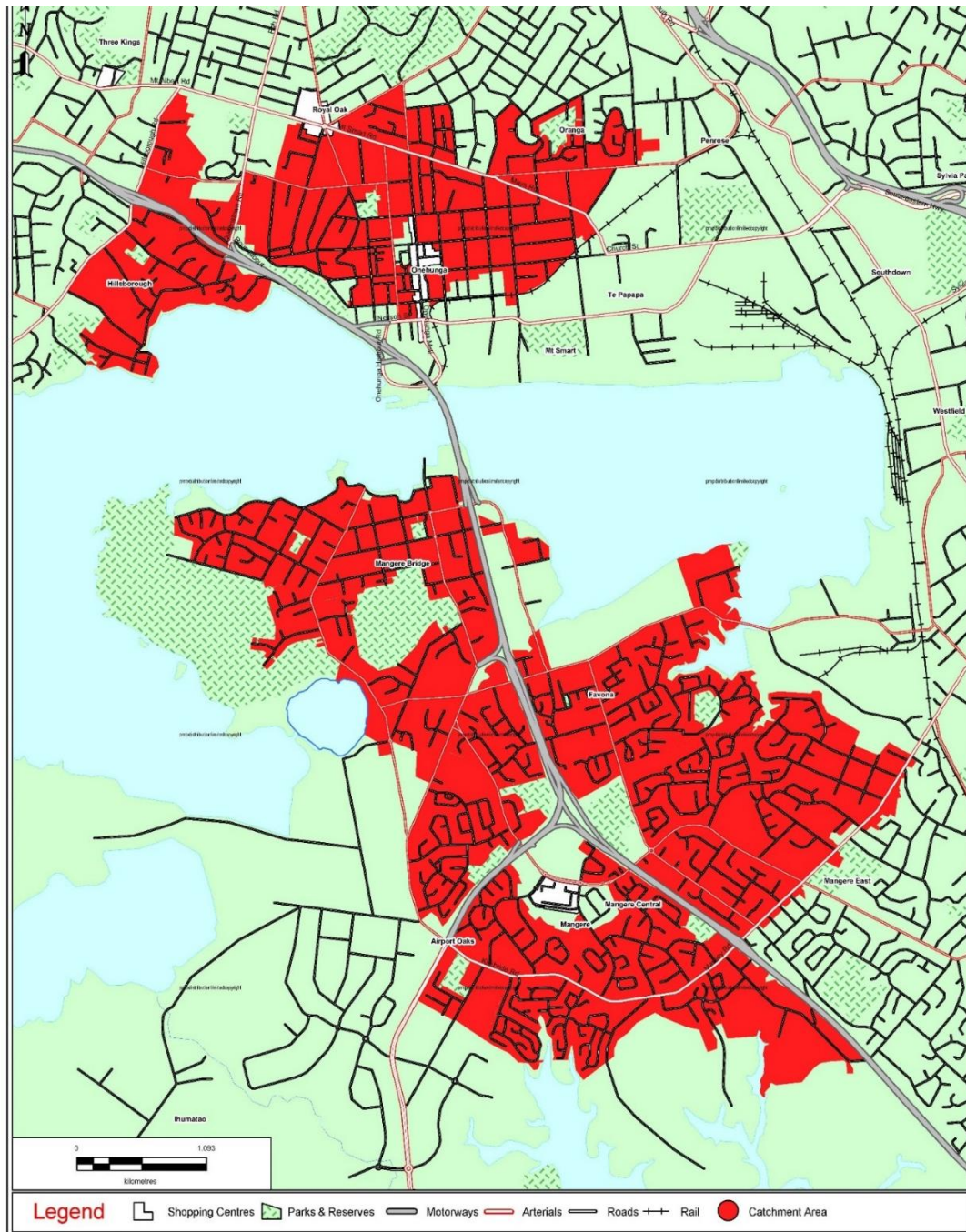


Figure 1: Newsletter delivery area – highlighted in red

4.4 Baseline features of the new bridge

The Project Reference Group established a set of baseline requirements for the replacement bridge in 2012 – essentially outlining minimum design parameters for the replacement bridge.

These baseline features have been updated and refined as a result of community feedback and project progress. The 2014 baseline features were available on the website, at the open day and in the newsletter.

The baseline features (as at August 2014) include:

- A minimum width of 8m (previously 6m)
- Wider bays of up to 12m to accommodate fishing and other recreational activities
- Abutments to be retained in the same location as the existing bridge
- New bridge will curve to the east of the existing bridge (towards the motorway bridges)
- High enough for small boats to access underneath
- Allowance for an opening span for larger vessels if required in future
- Clear sight lines across the bridge
- Good lighting to be provided
- Safe railings to be designed
- Rubbish bins to be provided
- A design that reflects the character and history of the bridge and surrounding area.



Images: Design concepts for the bridge arch and bays – as shown to the community

5 Information open day

5.1 Format

The 2014 information open day was held on Saturday 26 July from 10am-4pm. The information day was very well attended with over 800 people of all ages attending. The Old Mangere Bridge Project and the East West Connections Project (a separate project in the area) shared the facilities to make it a larger and more attractive and informative event for the community.

A marquee containing project information was erected at the Waterfront Road Reserve, just at the end of the causeway to Old Mangere Bridge. A sausage sizzle, coffee, soup, bouncy castle and face painting were provided for entertainment.

This provided an excellent family friendly atmosphere where people could learn more about the Old Mangere Bridge Replacement Project as well as wider transportation opportunities from the East West Connections Project.



Images above: Information open day – 26 July 2014

5.2 Information presented

Seven poster information boards were presented covering the following topics:

- Baseline features of the new bridge
- How community feedback has been used
- 2012 school workshops
- The history of the bridge
- Design impressions of the new bridge (what it may look like and key features)
- Iwi values and artist involvement

- Timelines going forward

To supplement the display posters, a 12m long ‘interactive’ display made of canvas was laid out along the floor of the marquee. The aim was to actually show people just how wide 8m of the bridge will be and how wide it will be with the bays extending it to 12m wide. Props were used to help demonstrate perspective – including two mannequins, a bicycle and fishing rods.

A copy of the display material is attached as Appendix 5.

In addition, Auckland Transport and the Transport Agency utilised the other half of the marquee to share and gather information about the East West Connections Project.

There were two key mechanisms for gathering feedback on the day:

- A huge interactive wall poster (similar to 2012) which utilised icons that people could write on and stick on the wall under the heading “Your thoughts?”
- A formal feedback form which was available for people to write comments on or take away and post back.

In addition, the email address oldmangerebridge@nzta.govt.nz was available for people to contact the project team or they could write to the Transport Agency using traditional post.



Images: The interactive feedback wall in action and complete at the end of the day

6 Feedback received

6.1 Feedback methods

There were four feedback methods available to the community to provide feedback – interactive wall at the open day, feedback form, email or telephone. The graph below in Figure 2 shows the popularity of the feedback methods.

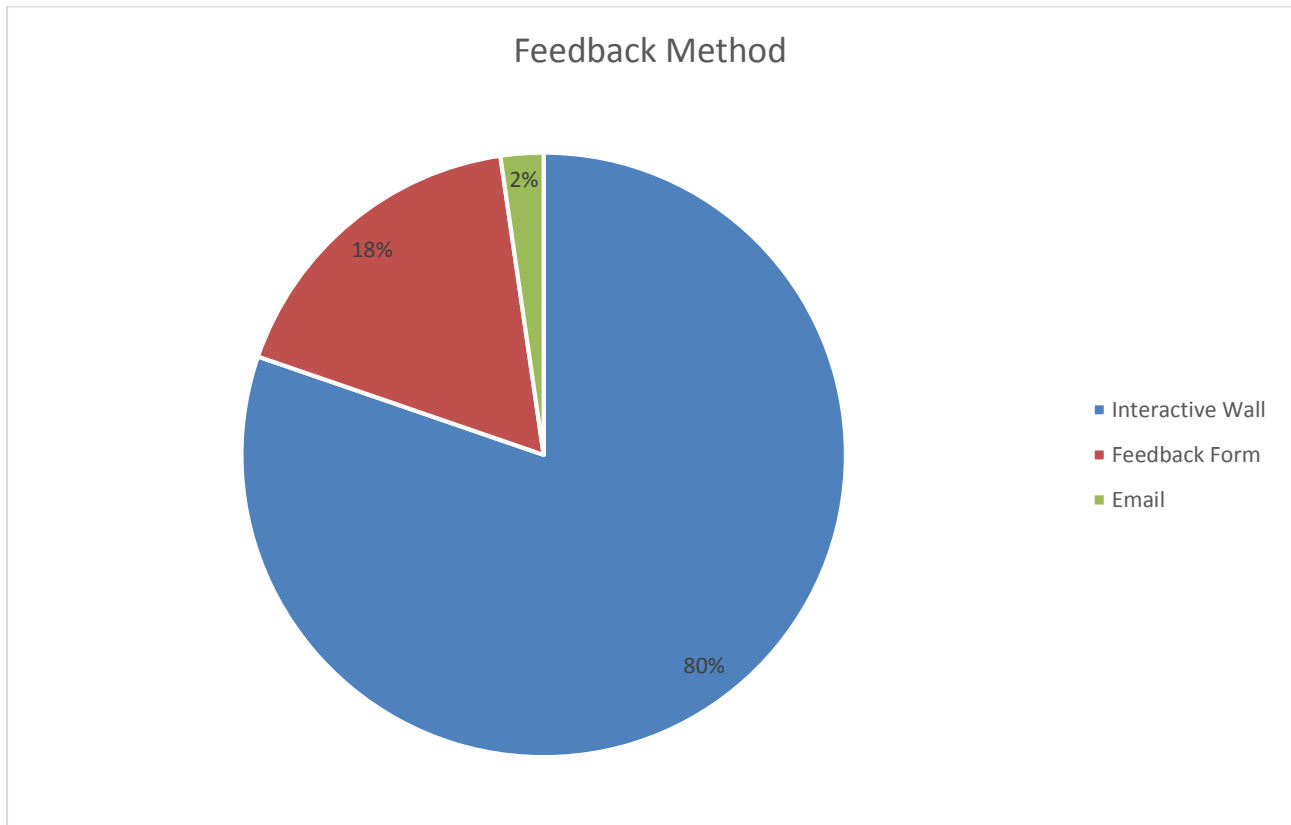


Figure 2: Feedback methods selected by respondents

The most popular feedback method was the interactive feedback wall, where community members could write a comment and stick it on a large poster titled 'Your thoughts?' There were 175 items of feedback placed on the wall during the six hours of the information open day. This feedback method was also very popular during the 2012 engagement.

The poster utilised sketches from the design and highlighted the key features of the arch and bays. There were 12 pre-prepared icons which included the arch, the bays, fishing, walking/family outings, cycling, running, boating, lighting, rubbish bins, seating, safety and history. In addition, there were blank icons which people could write on.

The aim of the interactive feedback wall was to enable community members to become involved in the feedback process without the need to fill in a formal feedback form. In addition, it provided a snap shot of community values and views and enabled the community to immediately share their ideas and to build on the ideas of others attending the open day. The project team were available to

discuss feedback as it went up and community members also discussed their feedback with other community members and could see what others had written.

A total of 43 other items of feedback were received:

- 5 emails were received during the engagement period.
- 38 feedback forms were placed in the feedback box at the information open day.

6.2 Feedback topics

Feedback received was varied and covered a wide range of topics and opinions. All feedback has been reviewed and categorised into topics. The pie-chart in Figure 3 below provides a snapshot of the percentage of comments categorised by topic. These are further discussed below, with quotes from representative responses provided in italics to illustrate feedback.

Note that many of the feedback forms / emails commented about more than one topic, so the percentage in the pie-graph relates to the total number of comments received, not the total number of feedback forms / emails / interactive wall icons.

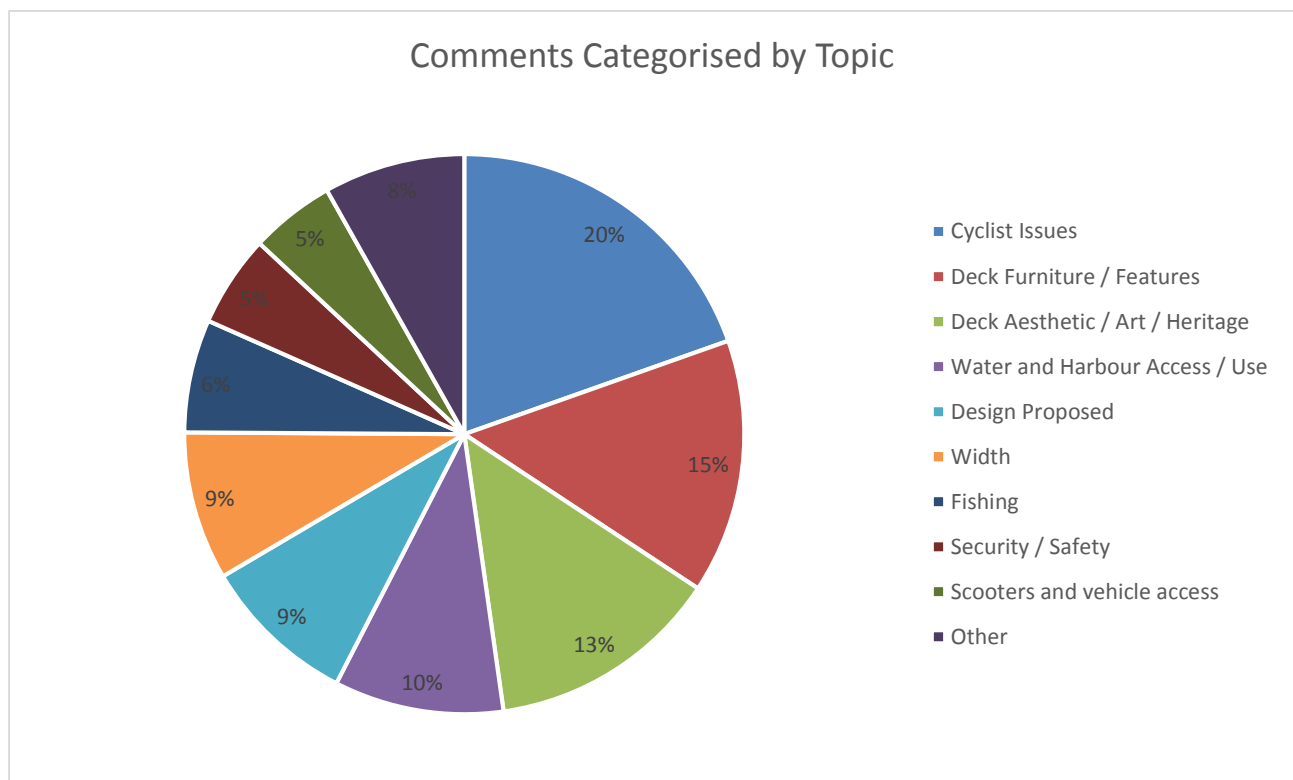


Figure 3: Percentage of Feedback Categorised by topic

The most common topic related to **cyclist issues**. Comments within this topic included the separation of cyclists from other bridge users (both for and against); the speed of cyclists and management of cyclist speed; the width of the bridge for cyclists and hazards for cyclists.

“Cyclists need to share the space, they do seem to think they have special rights”

“Keep cyclists separate”

“I want cyclists and pedestrians to share the space with tolerance and with a speed limit for road cyclists”

“Please can we have a dedicated cycle lane. I am a commuter cyclist so need to get from A to B quickly. It will be tricky to dodge pedestrians”

“Needs to be an easy flow through for all cyclists. Pinch points are a concern”

“Bays are a good idea, just looks like they may create pinch points with cyclist / pedestrians”

“Please be careful with ‘street furniture’ on the bridge. Small, obscure and hidden objects are a serious hazard for cyclists”

The second most common topic related to **deck furniture and practical features**. Comments within this topic were varied and included lighting, seating, rubbish bins, fishing facilities, shelter and safety features.

“Need good lighting on bridge”

“Seats particularly on western side view”

“A hand pump to bring saltwater to the deck for hand cleaning”

“It would be great to catch a fish and then BBQ as a family”

“Need shelters from rain and sun. A place to rest and take in the view”

“Please ensure that the new design incorporates a much safer ‘railing’ system than the current one”

The third most common topic related to the **deck aesthetic, art and heritage design**. Comments within this topic were varied and related to people’s ideas that could be incorporated into the design and deck features.

“All international planes east or west now fly up the harbour. What a great view it would be with the arch lit”

“‘Seating Panel’ to have black and white historical photos with info written with it”

“The settlers history should also be remembered and reflected in the new bridge”

“Love the idea of Hape & stingray included in design”

“Would be nice as a promenade”

“Be bold with the art work”

“This is the only opportunity to do something iconic - please make it memorable!”

The fourth most common topic related to **water and harbour access/use**. Comments within this topic included the desire to get closer to the water from the bridge; conflicts between fishermen / fishing lines and those using the water under the bridge; and access to the water upper harbour for vessels.

“Step/ramp down the platforms to be more sheltered and closer to the water similar to the viaduct area”

“An area that is safe for us paddlers not to get caught under the fishing hooks”

“Access to upper harbour for small boats and the annual power boats races”

“Make boat-ramp suitable for kayaks”

Other common topics raised included:

The design proposed – comments relating to the design features and aspects of the bridge itself.

“What a great asset for the community Love it!”

“Lots more bays - 3 is not enough”

“Thanks for listening to earlier feedback”

Width – a desire to see enough space available for activities to be undertaken comfortably and safely (note this sentiment is also reflected in the category relating to cyclists).

“Min width 12m - with Bays up to 16m – it’s only going to get busier with all the development”

“Effective width is 4m which I think is too narrow”

“8 Metres is too narrow to accommodate all the activities that are currently provided”

Fishing – desire for the space to fish comfortably and safely.

“Majority fishing on western side - enough room to sit fish and others to get past”

“Should encourage fishing on the bays and not on the bridge or only on certain times”

“Fisher people need space to cast behind them”

Security / safety – concern about anti-social behaviour.

“Safety cameras and CCTV for security and general observing community wellbeing”

“Regular Security checks to keep truancy control in the area”

Scooters and vehicle access – desire to manage scooters who use the bridge to cross and also a desire for a local traffic option, avoiding the motorway.

“Motorised scooter/moped management needed”

“Motorised scooters - growing number using old bridge - need to accommodate them as it is too dangerous for them to use motorway”

“Bridge should be a local road - no trucks - just cars going to Onehunga so they don't need to go on the motorway”

Other less common topics raised included maintenance issues associated with the current bridge (litter, toilets, lighting) and the desire to alleviate these in future; design for an opening span; the ability to host markets/events on/near the bridge; connectivity to cycle/walkways in the area and between the townships; and ability for safe/easy walking. .

6.3 Stakeholder feedback

Stakeholder responses are identified as responses representing a group of people or a particular interest.

Three written feedback forms were provided by stakeholder and community groups. These are summarised below.

Mangere Bridge Residents and Ratepayers Association – support the design presented and are pleased with the outcome. Thanked the Transport Agency for listening.

South Auckland Youth Arts Network – liked the designs. Would like access to the water considered, such as a mini-wharf, and artwork opportunities.

Otahuhu Mangere Youth Group – Would like the ability for events to encourage youth interaction and reduce anti-social behaviour in the area.

Project Reference Group (PRG) – The PRG meetings provided an opportunity to update key stakeholders and to obtain their feedback. The main feedback centred around the width of the new bridge – would 8 m be sufficient to accommodate the variety of uses; whether remnants of the old bridge could be retained as a memory of the old bridge; the layout of the bridge deck particularly as it related to cyclists and the safety of other users; the importance of reflecting the history of the area into the design; the importance of maintaining access during construction; and including CEPTD principles into the design.

7 Summary and analysis of feedback

The summary below provides an overview and analysis of the feedback received (which is detailed above in Section 6).

7.1 Context

- Old Mangere Bridge is considered by the community as a vital asset with access and recreational value provided for a diverse community of users.
- The aim was to inform the community about project progress and a wide advertising and newsletter area was utilised to share this information.
- A range of engagement and feedback mechanisms were employed for stakeholders and the community, including one community information open day.
- No specific feedback was sought during this engagement period. However, the community were invited to comment generally on the design concepts presented.

7.2 Key views and themes

Overall, the responses were positive to the design concept and information presented. Feedback indicated that the current design concept has addressed many concerns and requests made in previous feedback. The concepts of the bays and the arch were typically well received. Feedback generally focussed around features and tweaks that could be provided for or made to improve and enhance the design concept. One main issue, that of cyclist's needs and the needs of other bridge users, was still of concern to many respondents.

There was a very positive reaction to the Transport Agency's intent to incorporate the area's culture and heritage through artwork. Suggestions for a promenade style, arch lighting and various artistic and aesthetic suggestions illustrate the community's desire to see an iconic place developed that is more than just a bridge.

Tension between cyclists (in particular commuter cyclists) and other users appears high based on the feedback, with concerns are for the safety of all parties as well as convenience for cyclists. In general, respondents noted that cyclists are looking for a direct path across the bridge for which they do not need to slow down.

Respondents commented on apparent pinch-points that could occur near the bays and with street furniture, effectively narrowing the bridge and creating hazards for cyclists. Respondents requested additional width on the bridge to safely and comfortably accommodate all activities and/or dedicated cycle areas/lanes to address this concern. Conversely, a number of responses requested that the area be mixed-use, with no clear delineation for cyclists such that they will need to slow down as they cross the bridge.

In addition to pinch-points near bays, there were also some concerns as to whether bays would adequately provide for fishing, particularly casting, and whether or not they would indeed be used by people fishing instead of other narrower locations along the bridge (particularly the western side

which was noted as the favoured side). Concerns around the interaction of fishing and those in the water (such as waka and kayaks) were also raised.

The use of scooters and mopeds on the bridge was raised as a request to allow them use of the bridge given no practical alternative is available (motorway access is not allowed) and they use the bridge now anyway. Given it won't be possible to prevent them from using the bridge as they fit easily through bollards and walkways, safe provision for and management of them is suggested by respondents.

Further investigation of the three issues of cyclist tensions, operation of the bays and scooters is recommended as the design progresses to ensure these issues are actively addressed.

It is clear from the feedback that the community would like the bridge and surrounding area to become an important community asset. Requests for the provision of deck furniture and practical features such as lighting, seating, security CCTV, shelter and fishing facilities to make it a more safe and useable space that not only provides a connection across the harbour, but also a destination to enjoy the harbour and surrounds.

Overall, the common themes of the feedback can be summarised as:

- The current design concept has addressed many concerns and requests made in previous feedback and the community are positive about the design concept proposed.
- Consideration is required as to how to manage cyclists and other users to ensure all can use the bridge safely and enjoyably.
- The community are seeking a bridge that is a safe and useable space that is not only a connection, but also a valuable destination with a strong sense of culture, heritage and space.



Images: Residents give their feedback at the Information Open Day

8 How feedback will be used

The feedback received during the consultation period (July – August 2014) has been brought together and provided to inform the project team, including the Project Reference Group, Mana Whenua Roopu, design team, artists and wider project team. It will also be available to the wider public via the NZTA website.

The project team will review the feedback and look to incorporate it into the project design as appropriate.

8.1 Post Script

During 2015 the design of the bridge has been refined to incorporate stakeholder, community and Mana Whenua feedback in various areas including:

- Safety – provision of lighting and CPTED Principles;
- Deck furniture – form and position;
- Inclusion of bays for fishing;
- Inclusion of shared Mana Whenua stories, values and theming;
- Inclusion of community's and children stories;
- Re-use of parts from the old bridge to help preserve its history (where possible);
- Rubbish bins at both bridge abutments;
- Possible capture of treatment of stormwater via raingardens at the end of each abutment in response to Mana Whenua feedback.

9 Next steps

The design, incorporating feedback as appropriate, is due to be finalised in 2015.

No further community engagement events are planned during 2015 however ongoing engagement with Mana Whenua and other stakeholders will continue as required. Project updates will be made available on the Transport Agency website as the project progresses into the consenting and construction phases.

10 Appendices

Old Mangere Bridge Replacement

Appendix 1

Consultation Plan

Old Mangere Bridge Replacement Project - Public Engagement Plan 2014

Version History

Version	Changes Made By	Date of Changes
Version 1	Tania Reynolds and Rebekah Pokura-Ward	23-4-14
Version 2	Tania Reynolds and Rebekah Pokura-Ward	18 June 2014

1. Purpose of this Public Engagement Plan

This Public Engagement Plan has been developed for the Old Mangere Bridge Replacement Project for the second round of public engagement to be undertaken during early-mid 2014. It outlines the objectives of engagement and level of influence on decision making (the WHY and WHAT), WHO will be engaged, HOW engagement will be undertaken and (the WHEN) timeframes for engagement This engagement plan draws heavily on the engagement already undertaken as part of an earlier phase of the project in 2012, detailed below.

As a government organisation, the Transport Agency has requirements to engage the community in order to meet its statutory obligations under the LTMA, RMA and LGA. The Transport Agency also has internal policy commitments to engagement outlined in the HNO Environmental and Social Responsibility manual as well as the soon-to-be introduced Better Business Case process which is a principles-based approach that links strategy to outcomes and defines problems thoroughly before solutions are decided. The input from the community engagement will help inform BBC process.

This Public Engagement Plan is structured based on the International Association of Public Participation (IAP2) principles and core values. The IAP2 provides internationally recognised engagement best practice principles. IAP2 sets out a community engagement spectrum of participation depending on the decisions to be made and the associated level of influence the community can make to a project.

This Public Engagement Plan is a live document, meaning it is intended to be regularly updated to reflect progress and changes in approach as we work through the public engagement period.

2. Project Status

The Old Mangere Bridge Replacement Project is currently entering the design phase, with engineers and artists working together along with the PRG stakeholders to develop concepts for the design of the bridge. Following the engagement period, the final design will be completed and will progress to resource consent applications.

The Purpose of the project is:

- To ensure a safe connection for walking and cycling is provided across Manukau Harbour in the general vicinity of the existing Old Mangere Bridge.

3. Key Project Personnel

Below are the names and contact details of key NZ Transport Agency team members and advisors related to the public engagement programme.

Key Team Member	Role	Organisation
Sarah Cronwright	Project Manager (Development)	NZ Transport Agency
Ronnie Salunga	Project Manager (Delivery)	NZ Transport Agency
Gareth Mills	Transport Planner, Iwi Liaison and Artist's Panel Coordinator	NZ Transport Agency
Keelin Flynn	Communications	NZ Transport Agency
Rebekah Pokura-Ward	Planning and Engagement Advisor	Opus International Consultants
Tania Reynolds	Engagement Advisor	Opus International Consultants
John McNeil	Design Engineer	AECOM

Tammy Flavell	Communcations Advisor	CRISP Communications
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4. Brief History and Project Update

The Old Mangere Bridge was constructed in 1914 and is believed to be the oldest reinforced concrete bridge crossing a harbour in New Zealand. The bridge provided the primary vehicular access across the Manukau Harbour until the SH20 Mangere Bridge opened in 1983. With the opening of the new SH20 crossing the Old Mangere Bridge was closed to all traffic except pedestrians and cyclists.

The Old Mangere Bridge remains in use by pedestrians and cyclists and is a popular site for recreational fishers. The bridge also provides the most direct connectivity route between Onehunga and Mangere Bridge townships. The old bridge forms a vital part of the Regional Walking and Cycling Network, linking the recently completed SH20 Mt Roskill cycleway and Waikaraka Cycleway at the northern edge of the harbour, to Mangere Bridge Township and further south towards the Auckland International Airport.

The Old Mangere Bridge is in a critical structural condition requiring expensive maintenance to hold it at its current level of service. It is rapidly deteriorating and is not expected to provide a safe access for more than another 5 years.

Design parameters for the replacement bridge have been agreed and a draft Structures Options Report has been prepared by AURECON which identifies potential replacement options. All options follow the same alignment (slightly to the east of the existing bridge) and all share the same landing points which is general location of the existing OMB.

A concept design will be taken to the community for comment in July 2014. Following community feedback, the design will be further refined and finalised by the project team, ready for the consenting stage.

5. Previous Engagement

There is strong community attachment to the bridge and it is highly valued for its access, recreational and heritage values. This has been demonstrated throughout a number of projects in the area in recent years including the Manukau Harbour Crossing Project and the replacement boat ramp works. These

values and sentiments were further demonstrated through-out the initial public engagement period for this project, held from July to September 2012. A range of engagement and feedback mechanisms were employed during this period, including two community information open days that were very well attended. Overall, more than 800 people were estimated to have participated in the engagement and 705 written submissions were received.

Key themes from the feedback received can be summarised as:

- A desire to have the replacement bridge as wide as possible – similar to the current bridge width of 12 metres.
- Improved safety features need to be incorporated, including hand-railings and lighting.
- The ability to continue to undertake all current activities (including fishing, walking, cycling) in a safe and comfortable manner on the replacement bridge.
- Reflect (or preserve where possible) the history and character of the old bridge in the replacement bridge.

Further detail on the outcomes of previous engagement can be found in the *Old Mangere Bridge Replacement Community Engagement Report* dated October 2012.

By way of overview the community can be loosely be divided into 4 groups and to date have contributed to the project in a number of different ways:

1. The Project Reference Group (PRG) comprising key stakeholders and community representatives provided a governance function and established the design parameters. They also provided an ongoing sounding board (check and balance) to the project and conduit to the wider community (this is discussed further below).
2. Iwi – the project area is highly significant to Maori with around 13 tribes holding ancestral association with the area. Maori shared their values and heritage and the necessity of this being reflected in the design of the new bridge through artwork.
3. Stakeholders – there are a number of representative organisations and interest groups in the area. Stakeholders is defined by the project as any group or body representing the views of an interest or group.
4. Local Communities of Mangere Bridge and Onehunga- these comprised residents and local businesses who shared their values, heritage and aspirations for their communities and the importance of the bridge connecting communities and links to the past. They shared their stories and told the

project team what they wanted the new bridge to be used for and what it could look like. Their feedback influenced the form and function of design of the new bridge.

6. Public Engagement Objectives

The **goal** of the Public Engagement for Old Mangere Bridge is to provide key stakeholders and the community with the opportunity to be informed about the project and for them to be able to provide feedback to the team about specific project design elements.

The **promise** that is being made through the Public Engagement Plan is that the Transport Agency will keep stakeholders and the community informed throughout the project, listen to and acknowledge concerns and aspirations, and provide feedback on how stakeholder input influenced our decisions.

The **objectives** of the engagement are:

General

- Listen to and acknowledge all feedback received and provide clarity as to how feedback has been taken into account in the development of the project.
- Ensure communication materials are easy to understand, timely and accessible.
- Allow enough time for stakeholders and the community to contribute meaningfully.
- Ensure that the principles and core values of the International Association of Public Participation (IAP2) are met throughout all stages of consultation.
- Meet the requirements of the Land Transport Act 2003 and Local Government Act 2002 as well as the requirements of the Resource Management Act 1991 in relation to consents which will be applied for.
-

7. Level of Engagement

The level of engagement (i.e. level of influence on project decision making) will vary between the various ‘communities’ affected by or interested in the project. It is important to note that up until this point the community have had considerable influence on project decision making particularly in relation to the location, function and form of the new bridge. Now that these decisions have been made the focus will be on developing design concepts and updating the community on progress. There may be some opportunity for the community to comment on the design concepts (TBA).

- Local community will be engaged generally at the ‘inform’ level of the IAP2 spectrum
- The PRG will be engaged at the ‘involve’ and ‘collaborate’ levels of the IAP2 spectrum
- Iwi, through the manawhenua roopu, will be engaged at the ‘involve’ and ‘collaborate’ levels of the IAP2 spectrum
- Stakeholders will be engaged generally at the ‘inform/consult’ level of the IAP2 spectrum

For this phase of engagement, topics may include:

- Update on project progress.
- How the community has informed the project to date.
- Where the project will go from here.
- Show key themes and values that are to be incorporated into the design.
- Seek thoughts on design concepts.

8. Key Communities of Interest and Stakeholders

The following table identifies who we will be engaging with, the level to which they will be engaged on the project and method of engagement.

Stakeholder Group	Level of Engagement	Method
NZ Transport Agency	Involve	Reference Group
Auckland Council	Involve	Reference Group
Auckland Transport	Involve	Reference Group
Historic Places Trust	Involve	Reference Group
Mangere-Otahuhu Local Board	Involve	Reference Group

Stakeholder Group	Level of Engagement	Method
Maungakiekie-Tamaki Local Board	Involve	Reference Group
Iwi	Involve	Reference Group; design panel and mana whenua group
Auckland Harbour Master	Involve	Reference Group (non-attending)
Ports of Auckland	Involve	Reference Group (non-attending)
The Onehunga Enhancement Society	Inform - Consult	
Cycle Action Auckland	Inform - Consult	
Walk Auckland	Inform - Consult	
Aotea Sea Scouts	Inform - Consult	
Manukau Cruising Club	Inform - Consult	
Mangere Bridge Residents and Ratepayers Association	Inform - Consult	
Mangere Bridge Village Business Association	Inform - Consult	
Onehunga Business Association	Inform - Consult	
Manukau Harbour Restoration Society	Inform - Consult	
Waka Ama Club	Inform - Consult	
Manukau Harbour Forum	Inform - Consult	
Mangere Boating Club	Inform - Consult	
Manukau Boating Club	Inform - Consult	
CCS Disability Action	Inform - Consult	
Department of Conservation	Inform - Consult	
Forrest and Bird	Inform - Consult	
Local Schools – including Onehunga High, Onehunga Primary, Waterlea and Mangere Bridge Primary School	Inform - Consult	
Wider Community	Inform	

9. Delivery of Public Engagement Plan

The following provides an overview of the planned engagement. Further specific detail on how this will be achieved can be found in the implementation plan.

Who	What	How	When
Project Reference Group	Involve in key decision making process including selecting preferred design option.	Continue regular meetings.	Regular meetings
Mana Whenua	Involve in key decision making process including selecting preferred design option.	Continue regular meetings Especially in relation to coordination with the artists to share stories, values and history	Regular hui
Other Stakeholders	Inform on project progress and preferred design option; Consult on deck design elements and name for bridge.	Personal letter and copy of newsletter. Invitation to meet. Invitation to attend Open Day.	First week of July. Open Day 26 July 2014.
Wider Community	Inform on project progress and preferred design option; Consult on deck design elements and name for bridge.	Media Release. Newsletter in local papers. Open Day.	Newsletters second week of July. Open Day 26 July 2014.

Appendix 2

Key stakeholder list

Old Mangere Bridge Stakeholders List

Organisation	Contact Name	Title
Schools		
Waterlea School	Margaret Palmer	Principal
Onehunga High School	Deidre Shea	Principal
Onehunga Primary School	Viki Holley	Principal
Mangere College	John Heyes	Principal
Mangere Bridge Primary School	Judy Hanna	Principal
St Josephs Catholic School	Andy Thompson	Principal
Local Representatives, Residents & Business Associations		
MP for Maungakiekie	Hon Peseta Sam Lotu-liga	MP for Maungakiekie
MP for Mangere	Su'a William Sio	MP for Mangere
Maungakiekie-Tāmaki Local Board	Simon Randall	Chairman
Maungakiekie-Tāmaki Local Board	Chris Makoare	Deputy Chair
Mangere-Otahuhu Local Board	Lydia Sosene	Chair
Mangere-Otahuhu Local Board	Carrol Elliott	Deputy Chair
The Onehunga Enhancement Society	Jim Jackson	Chairman
The Onehunga Enhancement Society	John Yelverton	Secretary
Mangere Town Centre Business Association	Patricia Leach	Town Manager
Mangere Bridge Village	Carol-Anne Armitage	Manager
Onehunga Business Association	Amanda Kinzett	Town Manager
St Peters Anglican Church	The Rev'd Anne Priestley	Vicar
Mangere Bridge Scouts	Brian Corney	Regional Development Manager
Aotea Sea Scouts	John Heaton	Group Leader
Manukau Harbour Protection Society	John McCaffery	
Coastguard Northern Region Inc	Roly Linstrom	President
Manukau Cruising Club	Bob Roper	
Manukau Boating Club Inc	Brian Pilkington	Commodore
Manukau Yacht & Motor Boat Club	Brian Pilkington	Commodore
Mangere Boat Club	Lew Gibb	
Waka Ama NZ (Nga Kaihoe o Aotearoa Inc)	Ben Corbett	
MHRS - Manukau Harbour Restoration Society	Bronwen Turner	Deputy Chair
The Landing	Mark Alsen	Owner
Mangere Historical Society	Janet Presland	Member
Onehunga Fencible Society	Secretary	Secretary
Mangere Bridge Residents and Ratepayers Association	Roger Baldwin	Chairman
Mangere Bridge Residents and Ratepayers Association	Bill Kirk	
Onehunga Community News	Sally Webster	Marketing, Editorial & Online Editor
Onehunga Community News	Sheryn Werner	Editor in Chief
Manukau Courier	Anna Loren	
Wider Groups & Organisations		
Accident Compensation Corporation (ACC)	Stephanie Melville	ACC Media Team
Auckland District Health Board	Ailsa Claire	Chief Executive Officer
Cycle Action Auckland	Barbara Cuthbert	Chairperson
New Zealand Police	Tineke Lang	Community Constable
NZ Fire Service	Larry Cocker	Chief Fire Office
NZ Fire Service	Robert Watson	Senior Fire Risk Management Officer
St Johns Ambulance	Cam Craighead	Event Medical Services
Ports of Auckland	Matt Ball	Head of Communications
Ports of Auckland	Dee Radhakrishnan	Senior Communications Advisor
POAL Property	Craig Sain	General Manager Commercial Relationships
Kiwirail	Peter Reidy	Chief Executive
CCS Disability Action		
Vector Power	Shelley Watson	Group General Manager, External Relations
Vector Gas	David Thomas	Group General Manager Gas Trading and Metering
Telecom	Grant McTavish	
Vodafone	Craig Jones	Business Manager to CEO
Watercare	Andy Spittal	Principal Engineer
Transpower	Colin Thomson	Project Development Manager
Department of Conservation		
Harbour Master	Andrew Hayton	
Walk Auckland	Andy Smith	
Mahunga Dr Business Assn	Alex Holley	Manager

Appendix 3

Newsletter



July 2014

An update on the **new, Old Mangere Bridge**

The NZ Transport Agency and Auckland Council have been working over the last two years to further develop the design and layout of the *new, Old Mangere Bridge* – an exciting replacement for a much-loved community bridge. In this newsletter, you can find out more about the project to date and going forward, including:

- An update on project progress and the baseline elements of the design which is now underway.
- A summary of previous feedback from stakeholders and the community, including how this has been incorporated in the project.
- The timelines going forward to construction.
- An opportunity for you to let us know your thoughts.

Information Open Day – free family event

**Saturday 26 July from
10am–4pm, Waterfront Road
Reserve, Mangere Bridge**

Come along to learn more about the plans for the new bridge, discuss the bridge with the project team and give us your views on the concept and elements of the new bridge.

It will be an all-weather event, with sausage sizzle and a bouncy castle for children.

We would love to see you there!



Baseline features of the new bridge

Since our last Information Open Day in August 2012, the team have refined some of the key features of the bridge as a result of community feedback and project progress. The baseline features now include:

- A minimum width of 8m (previously 6m)
- Wider bays of up to 12m to accommodate fishing and other recreational activities
- Abutments to be retained in the same location as the existing bridge
- New bridge to be located to the east of the existing bridge (closer to the motorway bridges)
- High enough for small boats to access underneath
- Allowance for an opening span for larger vessels if required in future
- Clear sight lines across the bridge
- Good lighting to be provided
- Safe railings to be designed
- Rubbish bins to be provided
- A design that reflects the character and history of the bridge and surrounding area.

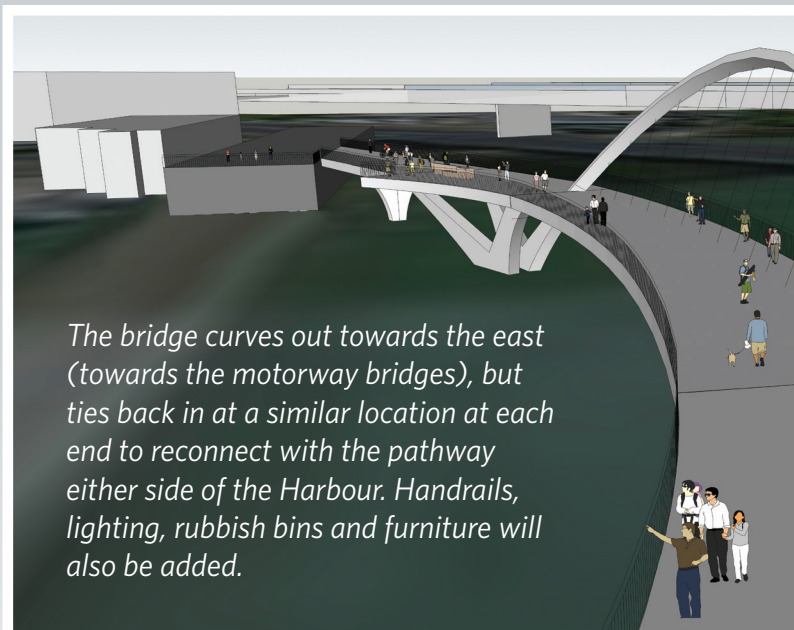
How community feedback has been used

The NZ Transport Agency initially sought community views in July and August 2012. There were five major themes of this feedback and the team have worked to ensure this has been incorporated into the new bridge design. The feedback themes and how they will be incorporated is summarised to the right.



Impressions of the new bridge

The images below and to the right show an impression of how the new bridge might look, with key features of the concept highlighted.



The bridge curves out towards the east (towards the motorway bridges), but ties back in at a similar location at each end to reconnect with the pathway either side of the Harbour. Handrails, lighting, rubbish bins and furniture will also be added.



An extra wide span is provided to allow small boats to pass easily underneath.



Feedback themes

How community feedback is incorporated

Keep the replacement bridge wide – similar to the current width

The minimum width has been increased from the original baseline of **6m** to a new minimum width of **8m**. In addition, wider bays of up to **12m** are proposed in key areas along the bridge to accommodate activities such as fishing.

Make sure it is safe – including much improved railings

Safety is a key factor in the bridge replacement and is being incorporated across a number of areas including sight lines through the bridge and new, vertical railing types.

Have it well lit

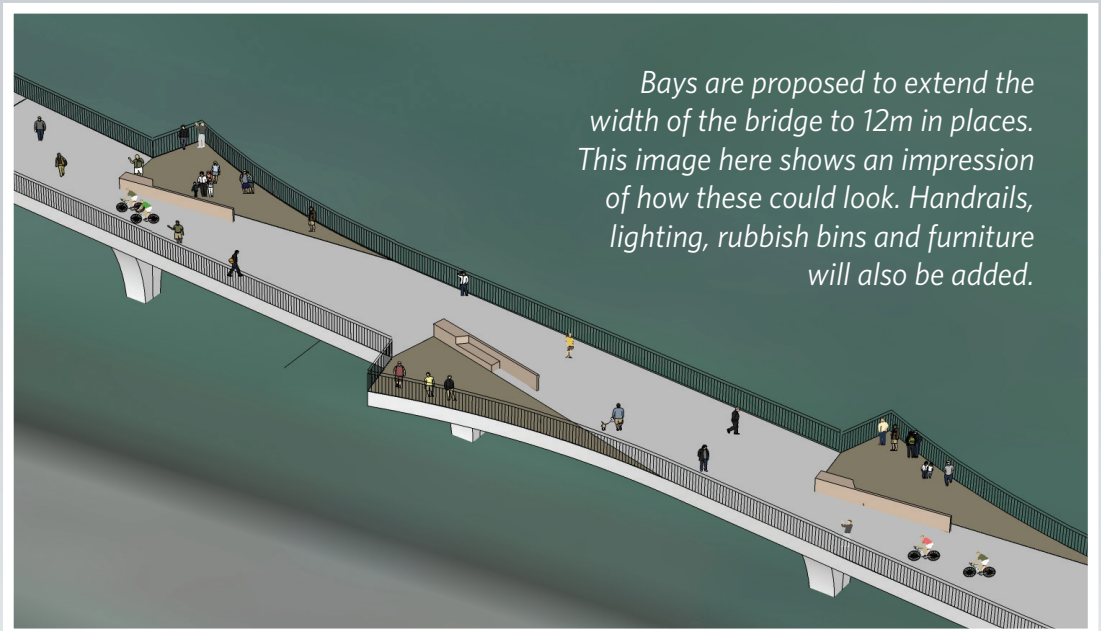
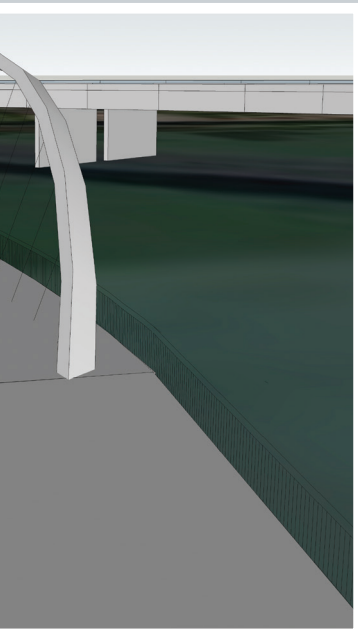
The design team are working to incorporate into the design good lighting that is functional and vandal-resistant. This will be a key feature of the new bridge.

Ensure that all of the activities currently undertaken can still be done so – both comfortably and safely

This feedback was presented to the Transport Agency in order to obtain approval for a wider bridge to accommodate the existing activities. The feedback and compelling community desire to use the bridge as not only an access way, but also a recreation space has meant the team can now accommodate the increased width as described above, meaning a variety of activities can be more comfortably and safely carried out.

Ensure the history and character of the bridge is reflected (or preserved where possible) in the replacement

The Transport Agency is working with a number of key stakeholders including Heritage NZ and local Iwi to understand key cultural, historic and character elements. Two artists have been commissioned to work with the design team to incorporate the area's history and values into the design through art.



proposed to enable easily underneath.

An arch is needed to give the bridge extra strength to support the wider span.

The new bridge will be at least 8m wide, all the way along.

Going forward from now...

The timeline below shows our estimate of how the new bridge will progress from now through to construction.



**Open Day -
we are here now**

Jul 2014

**Design - with artist
involvement**

Apr-Oct 2014

**Seek consents
from council**

Nov 2014



**Construction
starts**

2015

**Estimated construction
completion**

2016

More information and to share your thoughts

Come along to the Open Day on Saturday 26 July 2014 to find out more information. Alternatively, you can visit our website www.nzta.govt.nz/oldmangerebridge

We welcome your thoughts and comments on the concept for the new bridge and the information presented. Comments can be shared with us at the Open Day on 26 July 2014; or

You can email us at oldmangerebridge@nzta.govt.nz; or

You can write to us:

Old Mangere Bridge Team
NZ Transport Agency
Private Bag 106602
Auckland 1143



Appendix 4

Media release

JOINT MEDIA RELEASE: 11 July 2014

Communities asked to have their say on transport initiatives

Communities will soon get the chance to have their say about two significant transport projects in their area – the East West Connections and the replacement of the old Mangere bridge.

The NZ Transport Agency and Auckland Transport say there is an open invitation for people to attend three community days planned for later this month.

Two of them - at the Onehunga night market (Thursday 24 July, 6pm-10pm) and at Sylvia Park shopping mall near the foodcourt (Sunday 27 July, 10am-1pm) – focus on the East West Connections project. The third – at Waterfront Road Reserve, Mangere Bridge (Saturday 26 July, 10am-4pm) – will focus on both the East West Connections and the next stage of replacing the old Mangere bridge.

The Transport Agency’s acting Highways Manager, Steve Mutton, says the community days follow on from earlier commitments from the Agency and Auckland Transport to work with local people.

“We want to build on the great feedback we have already had from people to replace the bridge and carry that on into the East West Connections project. This is the latest step for us to ensure that we fully understand all the issues that people are facing when travelling in Onehunga, Mt Wellington, Otahuhu, Penrose, Mangere and East Tamaki,” Mr Mutton says.

Community input will help the Transport Agency and Auckland Transport develop their East West Connections programme to improve commuter and freight links, public transport and walking and cycling options over the next 30 years.

“We have already identified freight issues that need immediate attention in Onehunga-Penrose – that’s a key priority given the area’s importance for jobs and the Auckland and New Zealand economies. We will be working with stakeholders and the community in coming months as investigations progress for those improvements.

“But we are not losing sight of the issues people are facing in the wider area. The vibrant communities in the area are likely to experience a growth in the number of people who chose to live and work in them. Whether it’s congestion on the roads or a lack of bus shelters, we want to hear about it.”

Mr Mutton says that predicted growth will put additional pressure on the existing transport network.

“We’ve already identified the lack of reliable public transport between Mangere and Sylvia Park - there will be others. We want the conversation with local people now so that as we progress with improvements in Onehunga-Penrose, we can also continue to work with communities to address their issues,” says Mr Mutton.

The community day at Mangere Bridge on 26 July will also be a chance for people to see the proposed design for the new bridge connecting Onehunga and Mangere Bridge.

“The earlier feedback we obtained from both sides of the Manukau Harbour and from recreational users before work started was a catalyst for the project and guided the bridge design,” Mr Mutton says. “We’ve worked hard to integrate the community’s requests, and we’re optimistic that they will be pleased with our design when they see it.”

Mr Mutton says one of the key themes that shapes the design is recognition that the bridge is not only a harbour crossing, but a popular recreational space.

“This will enable people to continue to use the bridge as a choice fishing spot and provide safe access for pedestrians and cyclists.”

Some features of the original bridge will be retained, with the new structure curving towards the motorway bridge. It will be high enough for small boats to pass underneath. A wider span also means that some form of opening for larger craft is not precluded in future. Two artists have been commissioned to incorporate the area’s history and values into the design through art.

“Replacing the old bridge and the East West Connections are two very different projects with one similar outcome – helping the Transport Agency and Auckland Transport get the best solutions to improve the area’s transport network. We both clearly want to hear the views of people to help achieve that,” Mr Mutton says. **ENDS**

Additional details on both projects are available at the following websites:

<http://www.nzta.govt.nz/east-west> and <http://www.nzta.govt.nz/oldmangerebridge>

For further information contact:-

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Appendix 5

Information day displays



Welcome!

Information Open Day
10am-4pm for

**The new, Old
Mangere Bridge**

and

East West Connections





Baseline features of the new bridge

Since our last Information Open Day in August 2012, the team have refined some of the key features of the bridge as a result of community feedback and project progress. The baseline features now include:

- A minimum width of 8m (previously 6m)
- Wider bays of up to 12m to accommodate fishing and other recreational activities
- Abutments to be retained in the same location as the existing bridge
- New bridge will curve to the east of the existing bridge (towards the motorway bridges)
- High enough for small boats to access underneath
- Allowance for an opening span for larger vessels if required in future
- Clear sight lines across the bridge
- Good lighting to be provided
- Safe railings to be designed
- Rubbish bins to be provided
- A design that reflects the character and history of the bridge and surrounding area.



The New, Old Mangere Bridge.

How community feedback has been used

The NZ Transport Agency initially sought community views in July and August 2012. There were five major themes of this feedback and the team have worked to ensure this has been incorporated into the new bridge design. The feedback themes and how they will be incorporated is summarised below.

Feedback themes	How community feedback is incorporated
 <p>Keep the replacement bridge wide - similar to the current width</p>	<p>The minimum width has been increased from the original baseline of 6m to a new minimum width of 8m. In addition, wider bays of up to 12m are proposed in key areas along the bridge to accommodate activities such as fishing.</p>
 <p>Make sure it is safe - including much improved railings</p>	<p>Safety is a key factor in the bridge replacement and is being incorporated across a number of areas including sight lines through the bridge and new railing types.</p>
 <p>Have it well lit</p>	<p>The design team are working to incorporate into the design good lighting that is functional and vandal-resistant. This will be a key feature of the new bridge.</p>
 <p>Ensure that all of the activities currently undertaken can still be done so - both comfortably and safely</p>	<p>This feedback was presented to the Transport Agency in order to obtain approval for a wider bridge to accommodate the existing activities. The feedback and compelling community desire to use the bridge as not only an access way, but also a recreation space has meant the team can now accommodate the increased width as described above, meaning a variety of activities can be more comfortably and safely carried out.</p>
 <p>Ensure the history and character of the bridge is reflected (or preserved where possible) in the replacement</p>	<p>The Transport Agency is working with a number of key stakeholders including Heritage NZ and local Iwi to understand key cultural, historic and character elements. Two artists have been commissioned to work with the design team to incorporate the area's history and values into the design through art.</p>



The New, Old Mangere Bridge.

School workshops



In late 2012, the team visited Waterlea School, Onehunga Primary and Mangere Bridge School to gain a greater insight into how local children use the old bridge, what is important to them and how they would like to use the new bridge in the future.

Themes from the children's feedback are being incorporated into the bridge design. We loved the children's thoughtful questions and are exploring whether some of the interview questions can be "quoted" and included in the new bridge design.



Students were asked:

Imagine you could interview the Old Mangere Bridge - what would you most like to know?

- Dear bridge, what was it like sitting there through a storm?
- Do you ever get poked with hooks?
- Dear bridge, are you prepared for dying? (getting taken down)
- Do you feel you need a makeover?
- Would you like to be free finally?
- Are you bored being here?
- Those fish, do they tickle?
- How old are you? Are you being teased because of your age?
- Dear bridge do you get lonely at night?
- How many boats have crashed into you?
- Have you ever been tempted to kick anyone off you?
- Do you like it that stuff grows on you?
- How big is the biggest fish that ever swum under you?

What would make The New, Old Mangere Bridge useful for your friends and family? What would you like to do on the New Old Mangere Bridge?

- Underwater bridge so you can see fish and sharks
- Insert \$1 to activate pebble shooter gun
- Restaurant/café on bridge
- Underwater bridge that has rails and just after the rails a little feel pool with starfish you can touch
- Bird watching
- Rubbish bins to prevent rubbish and pollution
- Festivals
- Scuba diving
- Fishing spots along the bridge
- Underwater train
- Flying fox across water
- Air tunnel or teleportation pads or bouncy bridge
- Ice skating

What would the "BEST EVER" pedestrian bridge look like?

- Kiwi bridge - shaped like a giant kiwi
- Transparent section where you can see sea creatures
- Fishing decks on side of bridge
- Koru/spinning road
- Silver fern with decks and fishing spaces
- Half glass bridge
- Floating bridge
- Moving bridge
- Arch bridge
- Look like a mountain
- Taniwha
- Looks like lava is flowing
- Wiggly bridge in the shape of a 'K' (k for kiwi)
- NZ shape with lookout

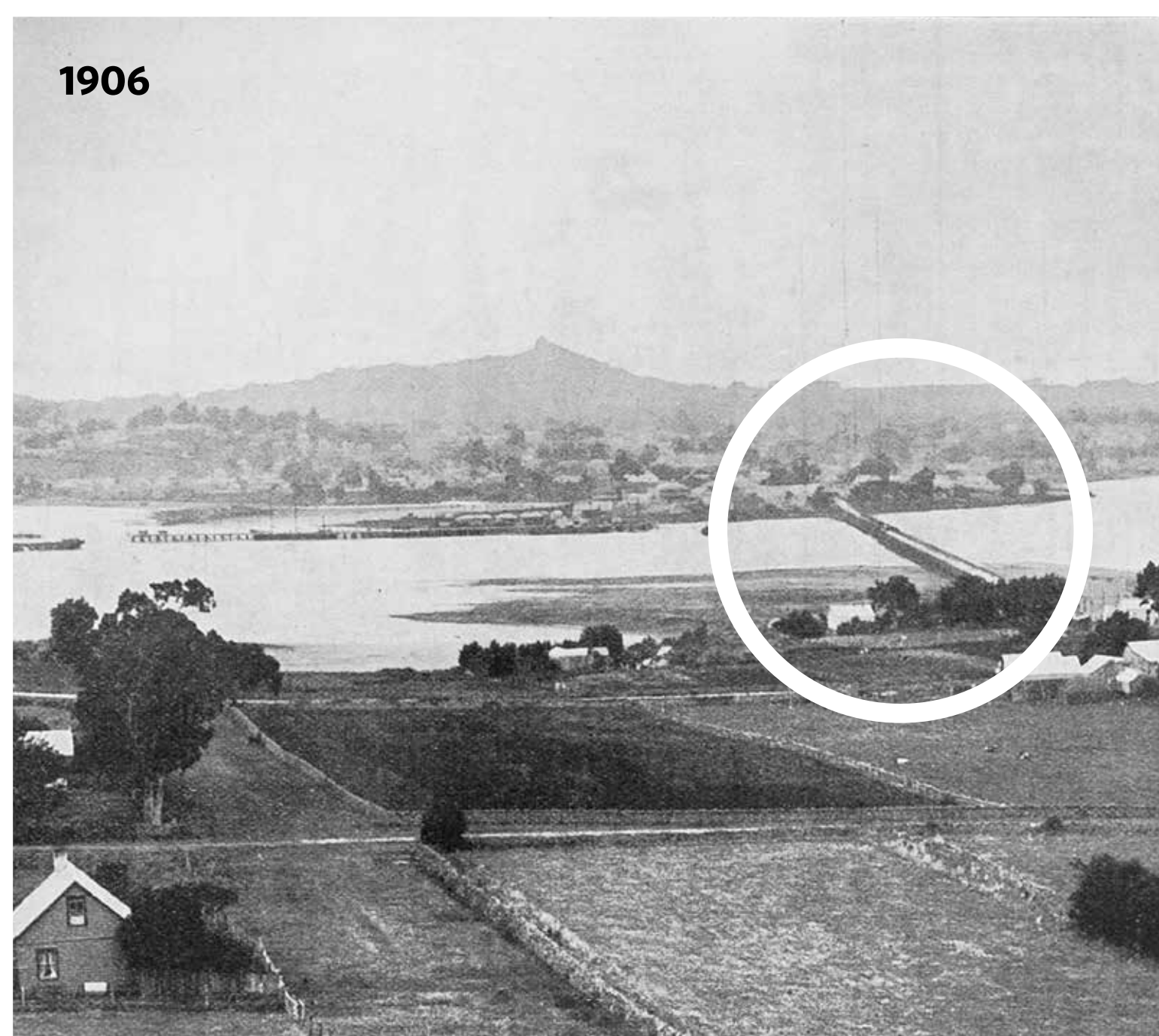


History of the bridge

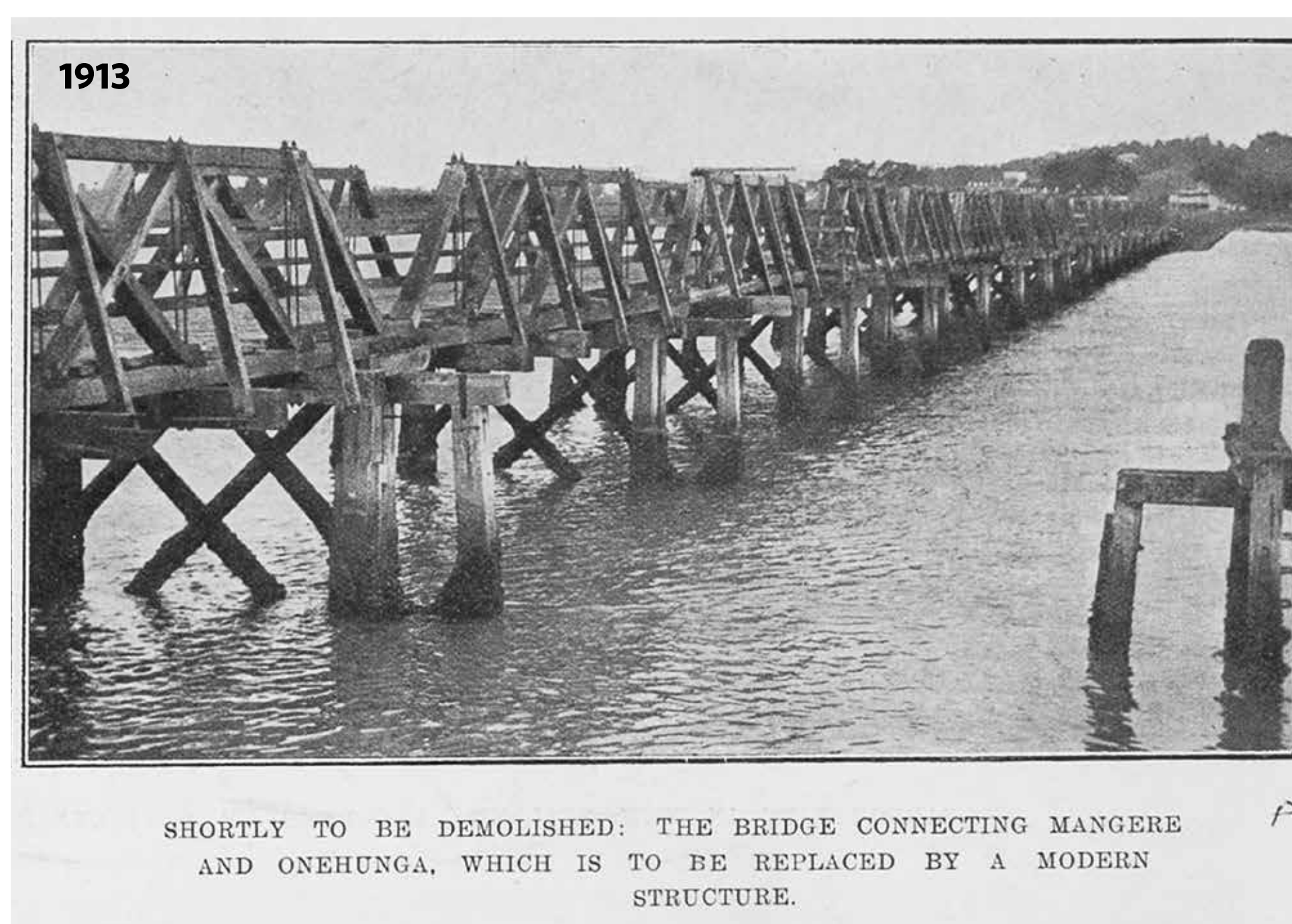
The first bridge built across the harbour in this location opened in **1875**. It provided an important link between the growing community in Mangere and the bustling port at Onehunga.

This original bridge was a narrow, one-way wooden structure that quickly deteriorated and was rather hazardous to walk over in high winds and stormy weather. It was also prone to attack by ship worms!

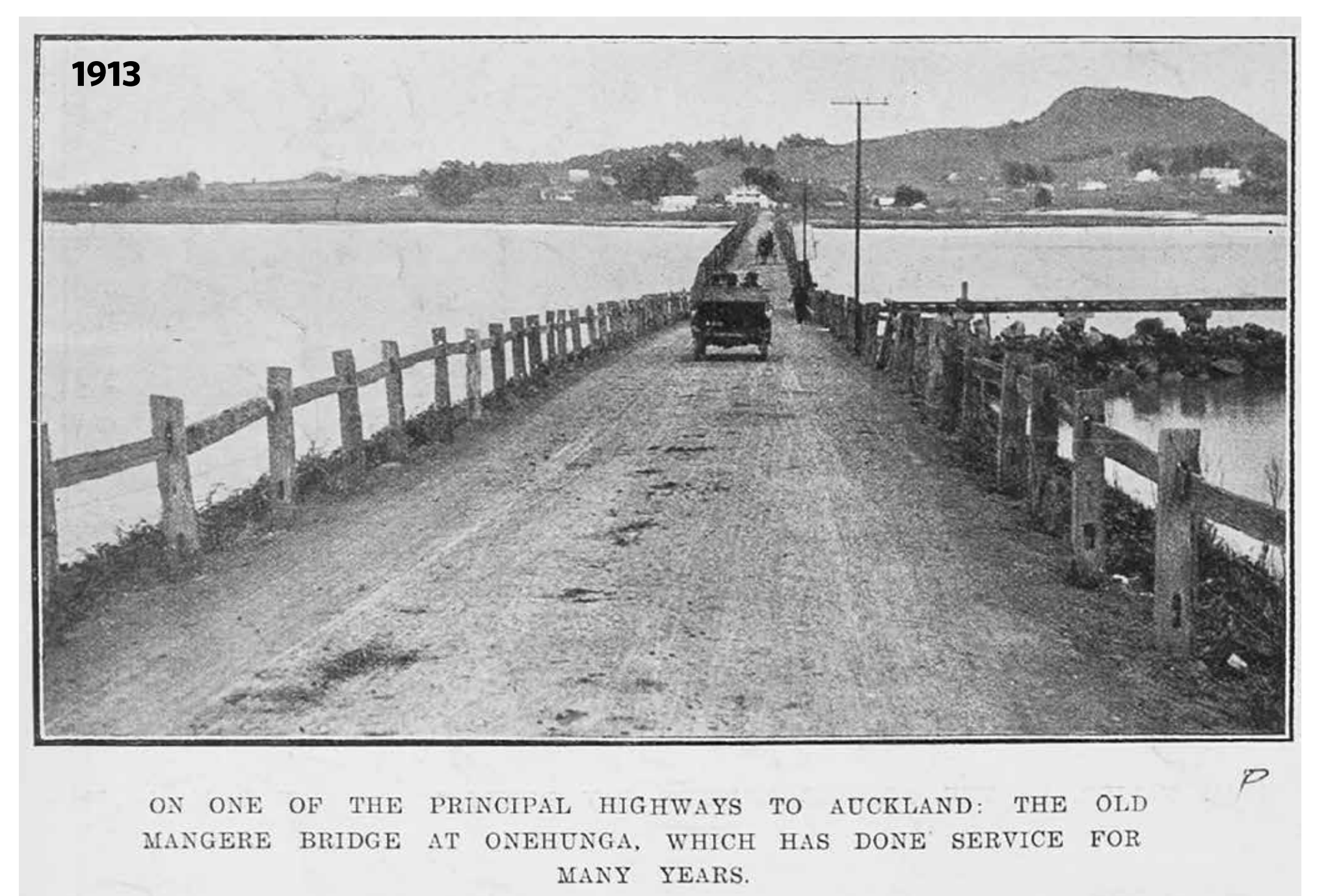
Small bays were later added so pedestrians could avoid animals and horse drawn vehicles. On calm days, the hooves of the animals on the bridge are said to have echoed loudly across the harbour.



Images above courtesy of Sir George Grey Special Collections/Auckland Libraries.



SHORTLY TO BE DEMOLISHED: THE BRIDGE CONNECTING MANGERE AND ONEHUNGA, WHICH IS TO BE REPLACED BY A MODERN STRUCTURE.



ON ONE OF THE PRINCIPAL HIGHWAYS TO AUCKLAND: THE OLD MANGERE BRIDGE AT ONEHUNGA, WHICH HAS DONE SERVICE FOR MANY YEARS.

The second Mangere Bridge (now known as the Old Mangere Bridge) officially opened in **1915**.

The bridge was designed by Mr R.F. Moore who also designed the Grafton Bridge. It cost approximately £22,000 - about \$0.5 million in today's terms.

This second bridge was built using reinforced concrete and it is believed to be the oldest bridge of this type crossing a New Zealand harbour.



Image courtesy of Mangere Bridge Library/Auckland Libraries, Footprints 01132.

In the **1980s** the bridge was in such bad condition a Bailey bridge (temporary bridge) over the existing bridge was built so it could stay open.

With the opening of a new motorway crossing over the Manukau Harbour in 1983, the Old Mangere Bridge closed to all traffic except for cyclists and pedestrians.

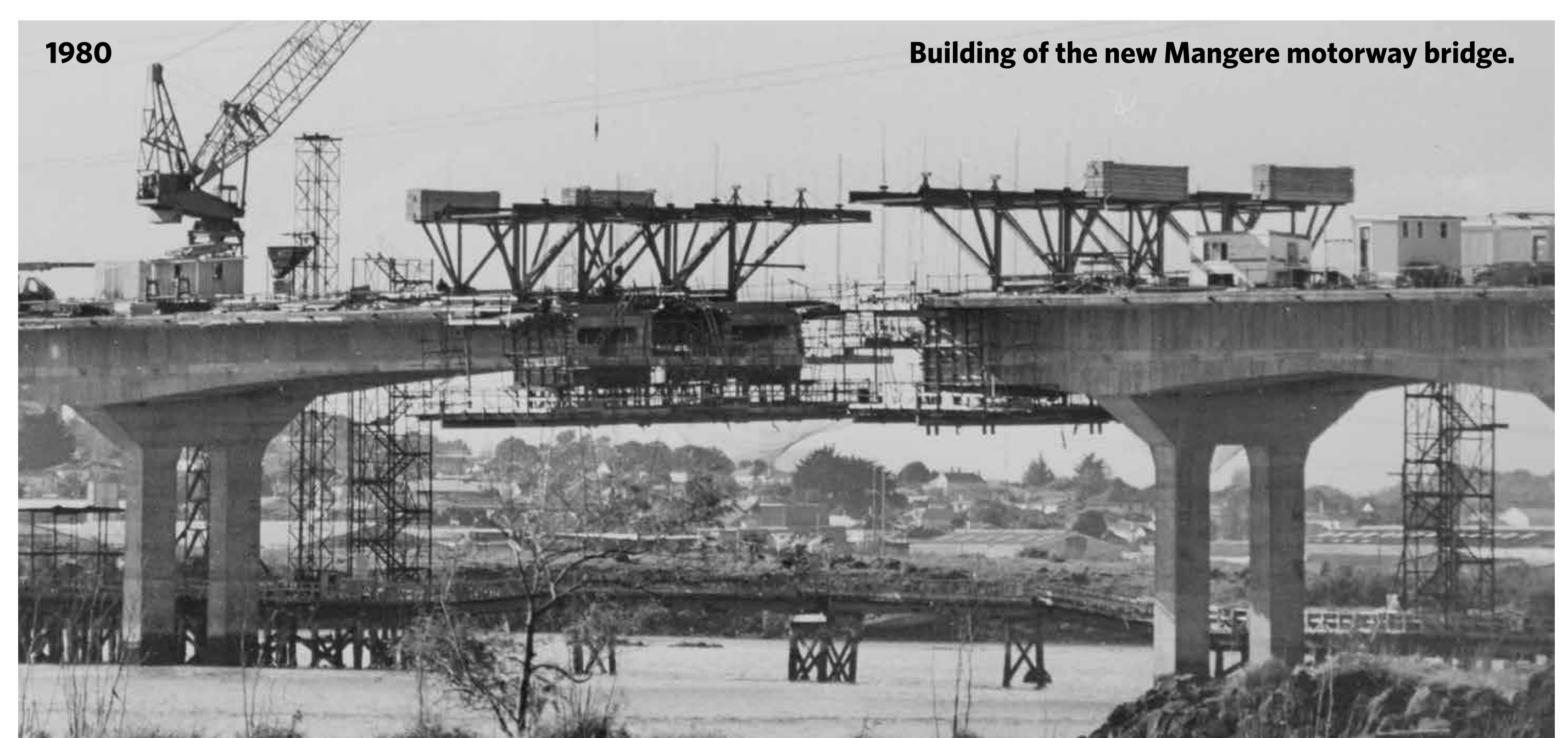


Image courtesy of Fairfax Media/Auckland Libraries, Footprints 00280.

Sources • Celebrating Mangere Bridge, Val Payne, 2005 • The Onehunga Heritage, Janice C Mogford, 1989



The New, Old Mangere Bridge.

Impressions of the new bridge...

The images below show an impression of how the new bridge might look, with key features of the concept highlighted.



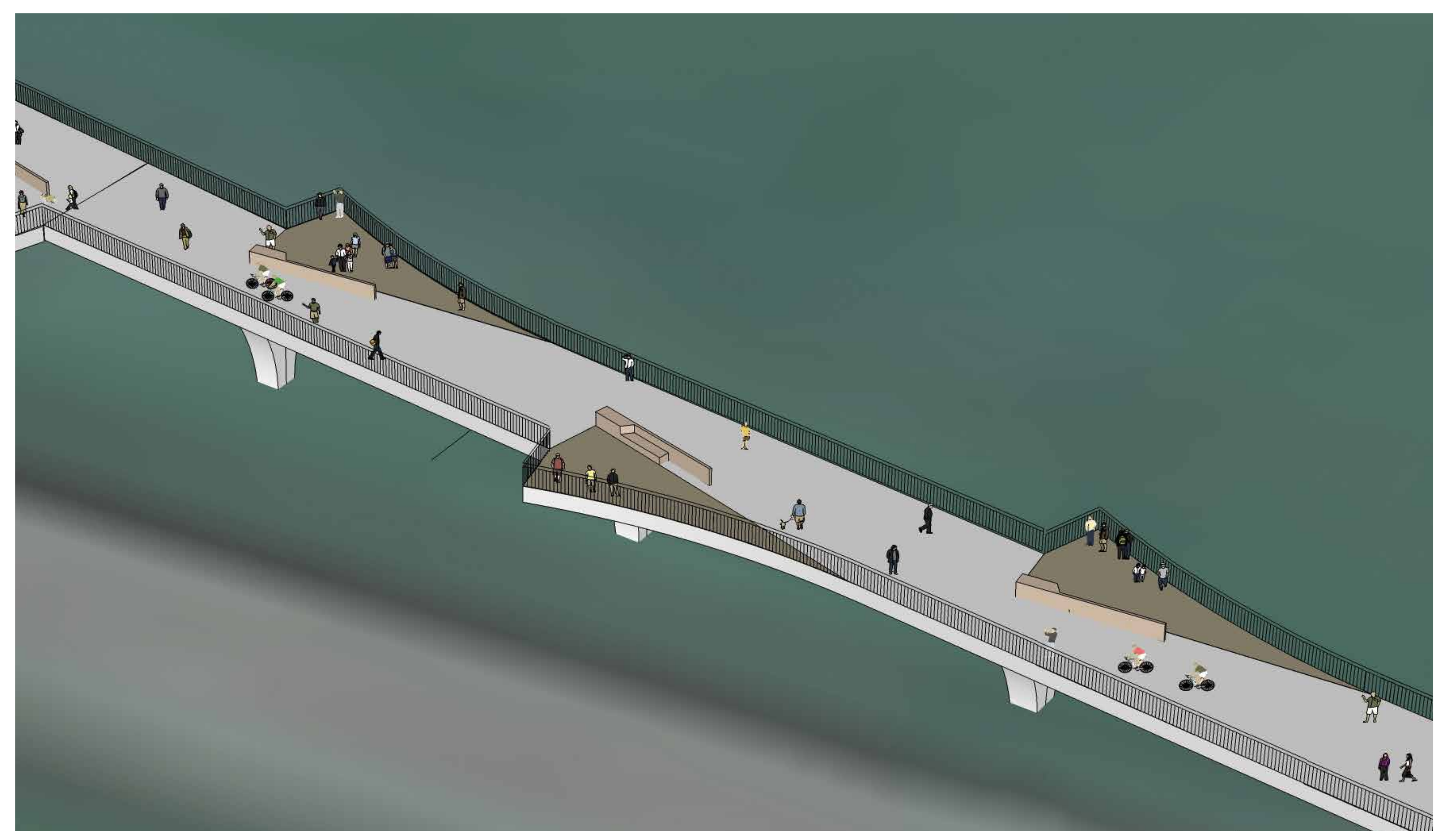
An extra wide span is proposed to enable small boats to pass easily underneath. The extra wide span (approximately 60m) will reconnect users to the upper harbour and enable better views.

The new bridge will be at least 8m wide, all the way along.

A single arch, approximately 12m high, on the eastern side of the new bridge gives extra strength needed to support the longer span underneath. The arch also aims to promote views to the west, provide some separation from the motorway bridges and provides a unique identity for the bridge to help give meaning and connection for the community.



The bridge curves out towards the east (towards the motorway bridges), but ties back in at a similar location at each end to reconnect with the pathway either side of the Harbour. Handrails, lighting, rubbish bins and furniture will also be added.



Bays are proposed to extend the width of the bridge to 12m in places to cater for a variety of activities. This image here shows an impression of how these could look. Handrails, lighting, rubbish bins and furniture will also be added.



Iwi values and artist involvement

From previous feedback, it was clear that culture and heritage are important aspects that need to be considered in the replacement bridge.

Taking an innovative approach, we are working with local iwi representatives and two local artists to identify agreed themes, values and stories that can be reflected through artwork.

Local artists Sunnah Thompson and Matthew Von Sturmer will then work with iwi and the design team to reflect these themes and values into the bridge design through artistic elements.

While their work isn't ready yet as they are just getting started, some of their previous work which you may recognise is shown here.



**MATTHEW
VON STURMER**



Matthew's work on the Pedestrian Bridges at Orangahina People's Park in Te Atatu



**SUNNAH
THOMPSON**



Sunnah's work at the Arataki Visitor's Centre in the Waitakere Ranges.



Sunnah's work on the Opanuku Walkway in Henderson



The New, Old Mangere Bridge.

Going forward from now...

The timeline below shows our estimate of how the new bridge will progress from now through to construction.



**Open Day -
we are here now**

Jul 2014

**Design - with artist
involvement**

Apr-Oct 2014

**Seek consents
from council**

Nov 2014

**Construction
starts**

2015

**Estimated construction
completion**

2016

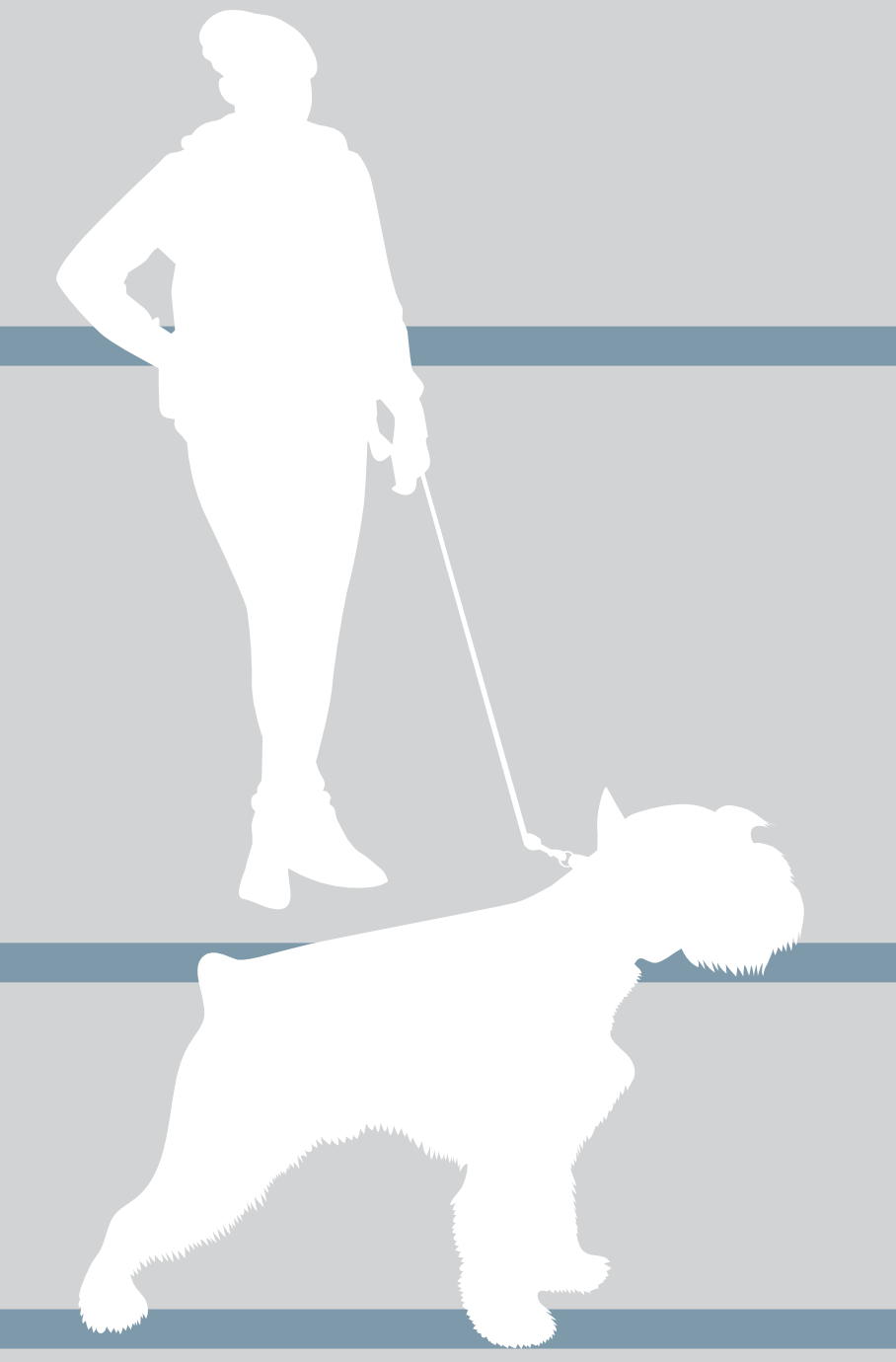




The New, Old Mangere Bridge.

Your thoughts?





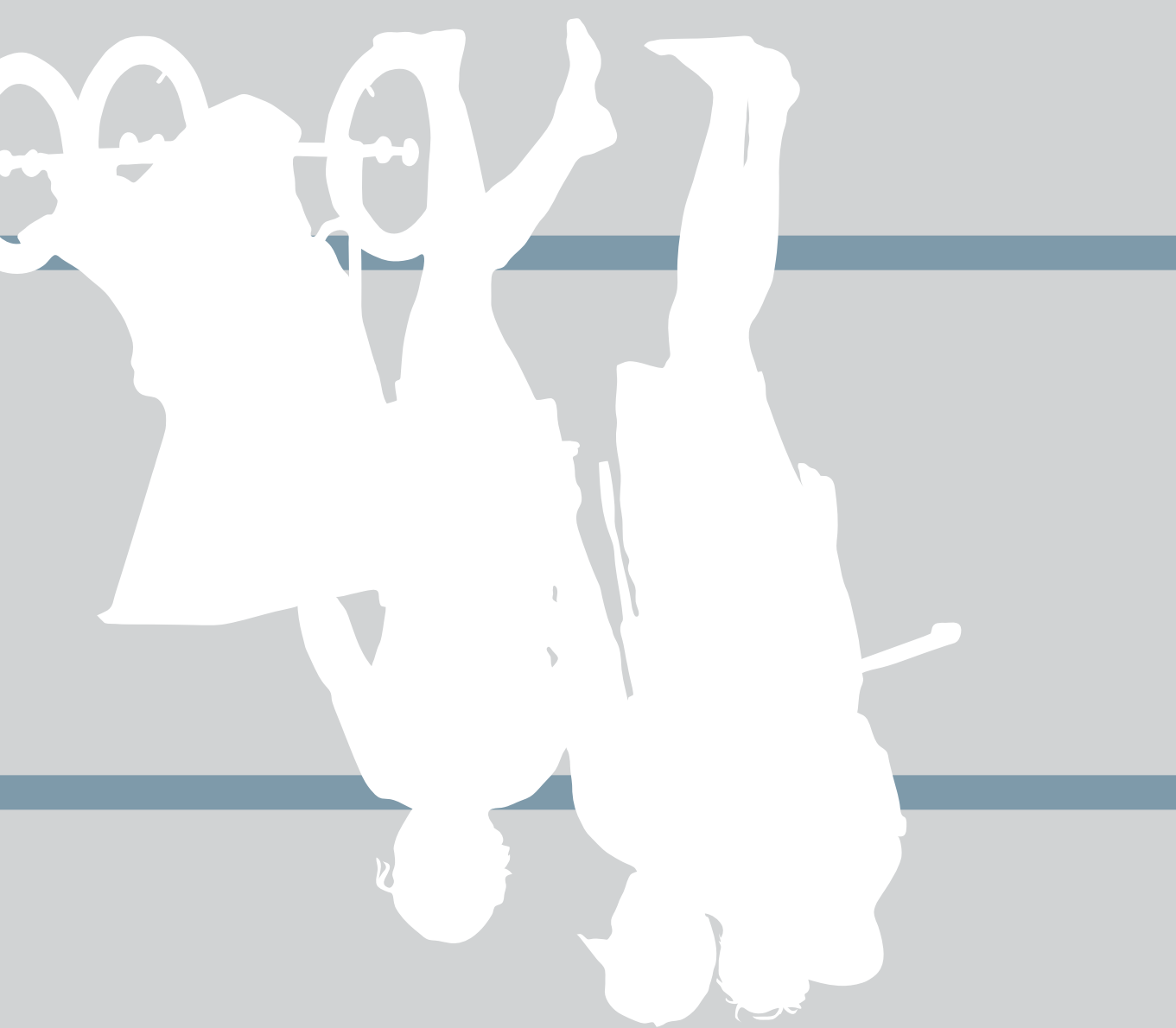
Get a sense of how wide the bridge will be...

8 metres minimum width all along - as shown in grey

8 metres minimum width all along - as shown in grey



8 metres minimum width all along - as shown in grey



8 to 12 metres in places - as shown in brown

Get a sense of how wide the bridge will be...



8 to 12 metres in places - as shown in brown

Proposed bays would extend the width from

8 to 12 metres in places - as shown in brown

Proposed bays would extend the width from

8 to 12 metres in places - as shown in brown



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