

KAPITI BRIDGE ABUTMENT AND FOOTPATH FINISH



VISUALISATION - KAPITI ROAD BRIDGE CROSSING (EAST SIDE OF EXPRESSWAY LOOKING WEST)

A1 REPRODUCTION SCALE
0mm
20
40
60
80
100

A3 REPRODUCTION SCALE
0mm
10
20
30
40
50

No.	Revision	By	Chk	Chk.V	Appd	Date
C	CERTIFIED ISSUE - REV C	VB		DS		18/07/14

Original Scale (A1)	Design	FB	18/07/14	Approved For Construction*
AS SHOWN	Drawn	VB	18/07/14	Date
Reduced Scale (A3)	Design Checker			
AS SHOWN	Design Checker			

* Refer to Revision 1 for Original Signature

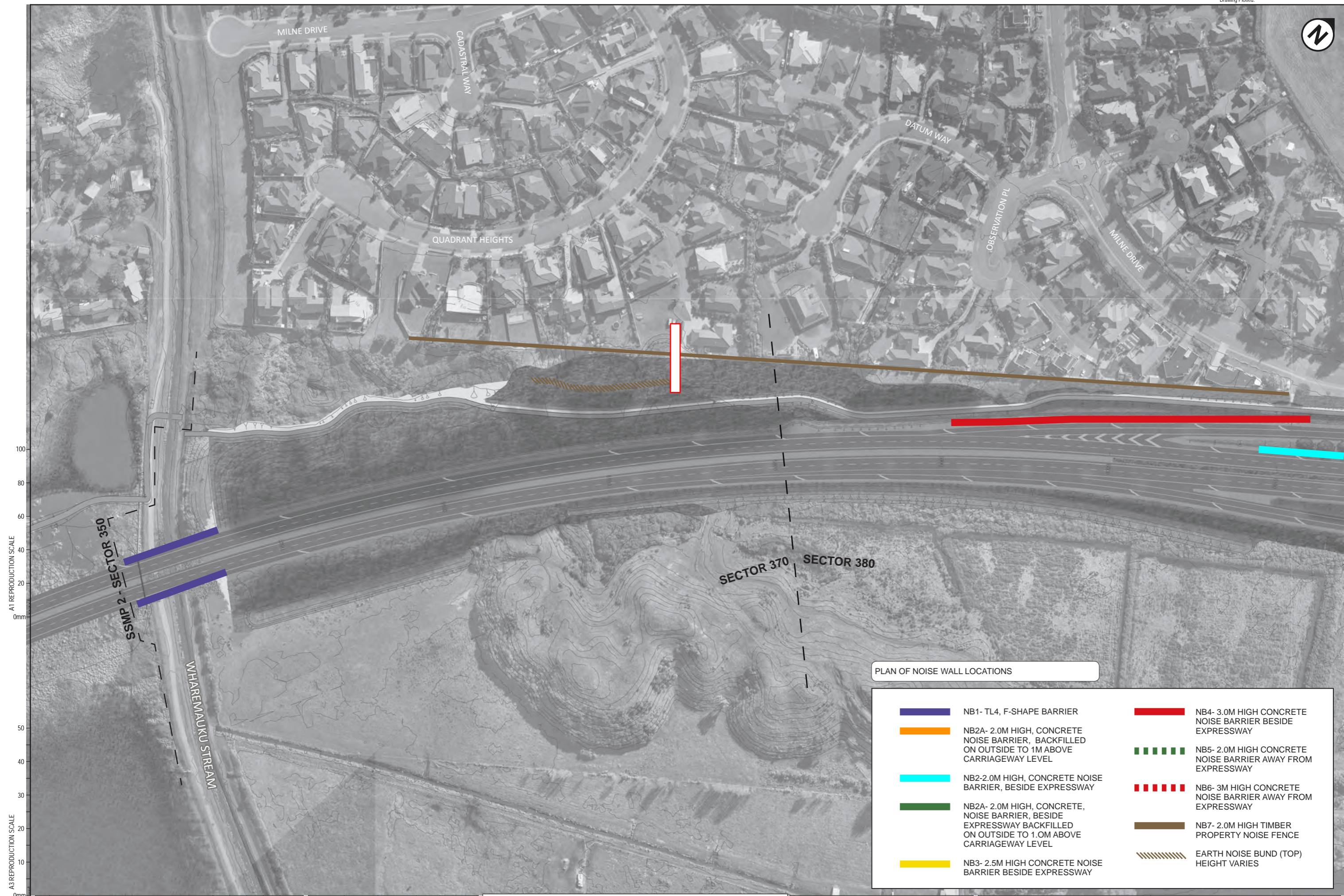
Project	SH1 MACKAYS TO PEKA PEKA EXPRESSWAY RP 1012/0.00 TO 1023/5.00
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Title	SSMP 3[350/370/380] - SHEET 13 KAPITI ROAD
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Drawing No.	M2PP-121-D-DWG-8802
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Rev.	C
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DETAIL DESIGN (DET)



PLAN OF NOISE WALL LOCATIONS

	NB1- TL4, F-SHAPE BARRIER		NB4- 3.0M HIGH CONCRETE NOISE BARRIER BESIDE EXPRESSWAY
	NB2A- 2.0M HIGH, CONCRETE NOISE BARRIER, BACKFILLED ON OUTSIDE TO 1M ABOVE CARRIAGEWAY LEVEL		NB5- 2.0M HIGH CONCRETE NOISE BARRIER AWAY FROM EXPRESSWAY
	NB2- 2.0M HIGH, CONCRETE NOISE BARRIER, BESIDE EXPRESSWAY		NB6- 3M HIGH CONCRETE NOISE BARRIER AWAY FROM EXPRESSWAY
	NB2A- 2.0M HIGH, CONCRETE, NOISE BARRIER, BESIDE EXPRESSWAY BACKFILLED ON OUTSIDE TO 1.0M ABOVE CARRIAGEWAY LEVEL		NB7- 2.0M HIGH TIMBER PROPERTY NOISE FENCE
	NB3- 2.5M HIGH CONCRETE NOISE BARRIER BESIDE EXPRESSWAY		EARTH NOISE BUND (TOP) HEIGHT VARIES

A1 REPRODUCTION SCALE
0mm 20 40 60 80 100

A3 REPRODUCTION SCALE
0mm 10 20 30 40 50

No.	Revision	By	Chk	Chk.V	Appd	Date
C	CERTIFIED ISSUE - REV C	VB		DS		18/07/14

Original Scale (A1)	Design	FB	18/07/14	Approved For Construction*
1:1000	Drawn	VB	18/07/14	
Reduced Scale (A3)	Design Check			Date
1:2000				



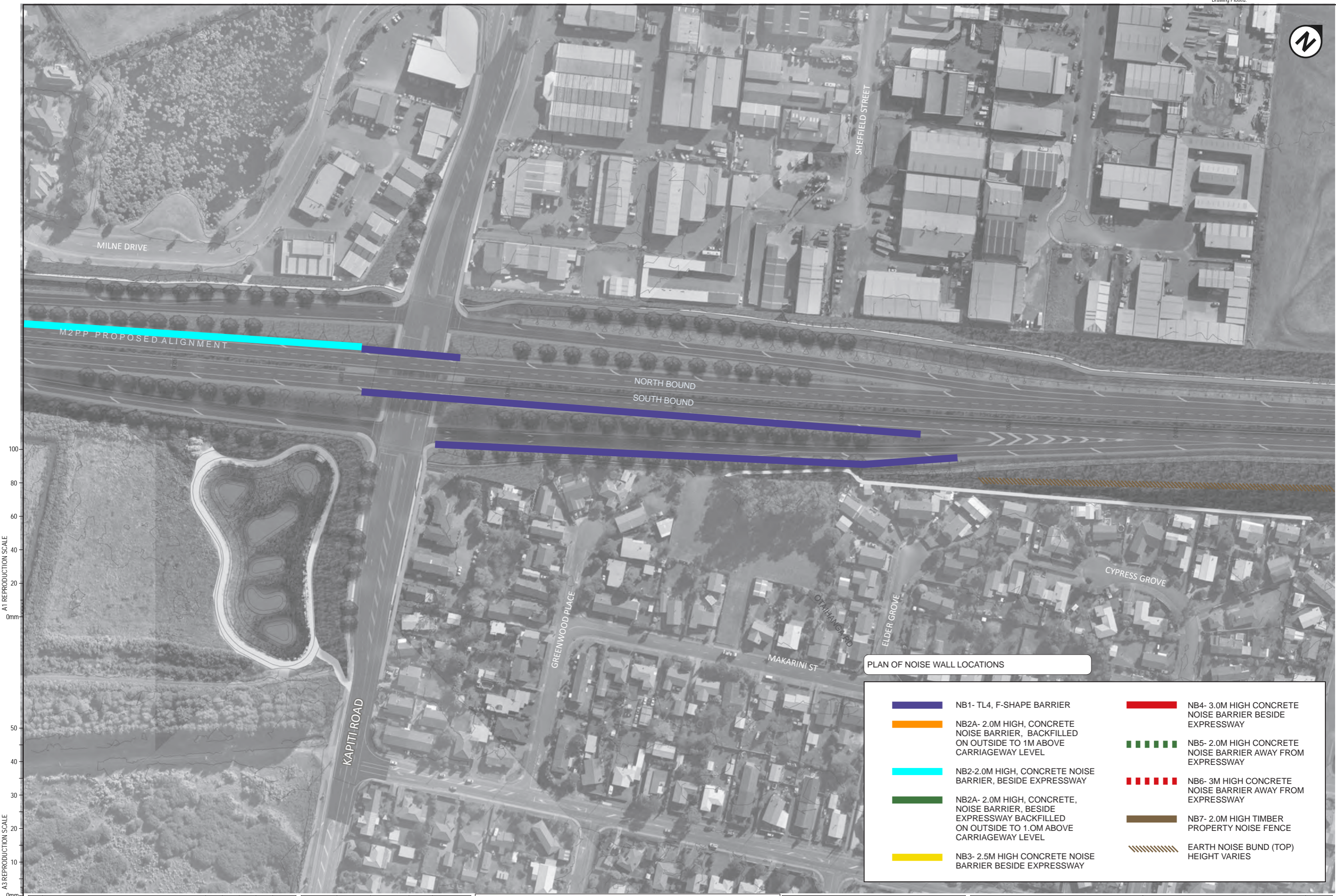
Project: SH1 MACKAYS TO PEKA PEKA EXPRESSWAY
RP 1012/0.00 TO 1023/5.00

Title: SSMP 5 & 6 - SHEET 14 NOISE WALL LOCATIONS











Drawing No: M2PP-121-D-DWG-8601

Rev: C

DETAIL DESIGN (DET)



PLAN OF NOISE WALL LOCATIONS

	NB1- TL4, F-SHAPE BARRIER		NB4- 3.0M HIGH CONCRETE NOISE BARRIER BESIDE EXPRESSWAY
	NB2A- 2.0M HIGH, CONCRETE NOISE BARRIER, BACKFILLED ON OUTSIDE TO 1M ABOVE CARRIAGEWAY LEVEL		NB5- 2.0M HIGH CONCRETE NOISE BARRIER AWAY FROM EXPRESSWAY
	NB2- 2.0M HIGH, CONCRETE NOISE BARRIER, BESIDE EXPRESSWAY		NB6- 3M HIGH CONCRETE NOISE BARRIER AWAY FROM EXPRESSWAY
	NB2A- 2.0M HIGH, CONCRETE, NOISE BARRIER, BESIDE EXPRESSWAY BACKFILLED ON OUTSIDE TO 1.0M ABOVE CARRIAGEWAY LEVEL		NB7- 2.0M HIGH TIMBER PROPERTY NOISE FENCE
	NB3- 2.5M HIGH CONCRETE NOISE BARRIER BESIDE EXPRESSWAY		EARTH NOISE BUND (TOP) HEIGHT VARIES

A1 REPRODUCTION SCALE
0mm
10
20
30
40
50
60
80
100

DETAIL DESIGN (DET)

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1:1000	Drawn	VB	18/07/14	
Reduced Scale (A3)	Design Verifier			
1:2000	Design Check			



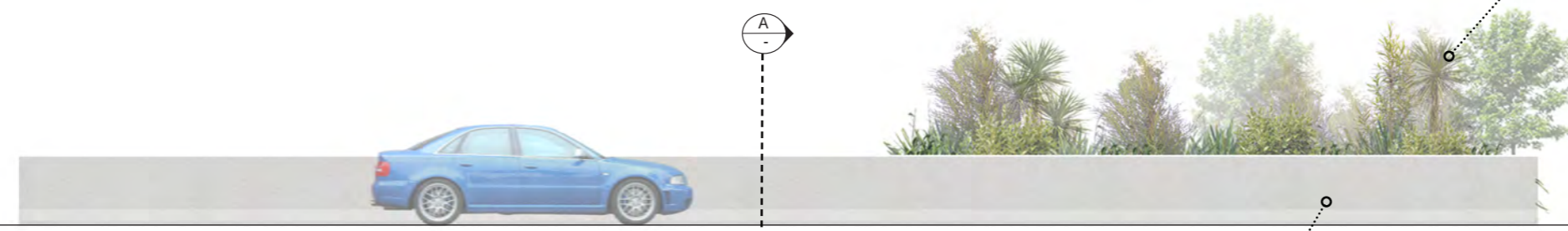
Project: SH1 MACKAYS TO PEKA PEKA EXPRESSWAY
RP 1012/0.00 TO 1023/5.00

Title: SSMP3[360/370/380]- SHEET 15
NOISE WALL LOCATIONS

Drawing No: M2PP-121-D-DWG-8602

Rev: C

ELEVATION 1 - NOISE WALL NB1(TL4)- EXPRESSWAY SIDE

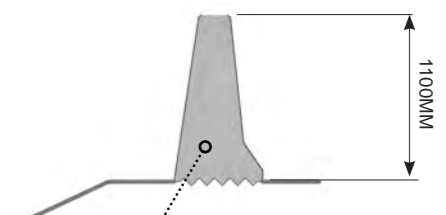


INDICATIVE PLANTING-FOR FULL PLANTING EXTENT REFER TO DWGS 8201-8203

SECTION A - NOISE WALL NB1 (TL4)

SCALE - 1:25@A3

RESIDENTIAL SIDE EXPRESSWAY SIDE



ELEVATION 2 - NOISE WALL NB1 (TL4)- RESIDENTIAL SIDE

TL4 FAIR FACED CONCRETE EXPRESSWAY BARRIER

TL4 FAIR FACED CONCRETE EXPRESSWAY BARRIER

INDICATIVE PLANTING-FOR FULL PLANTING EXTENT REFER TO DWGS 8201-8203

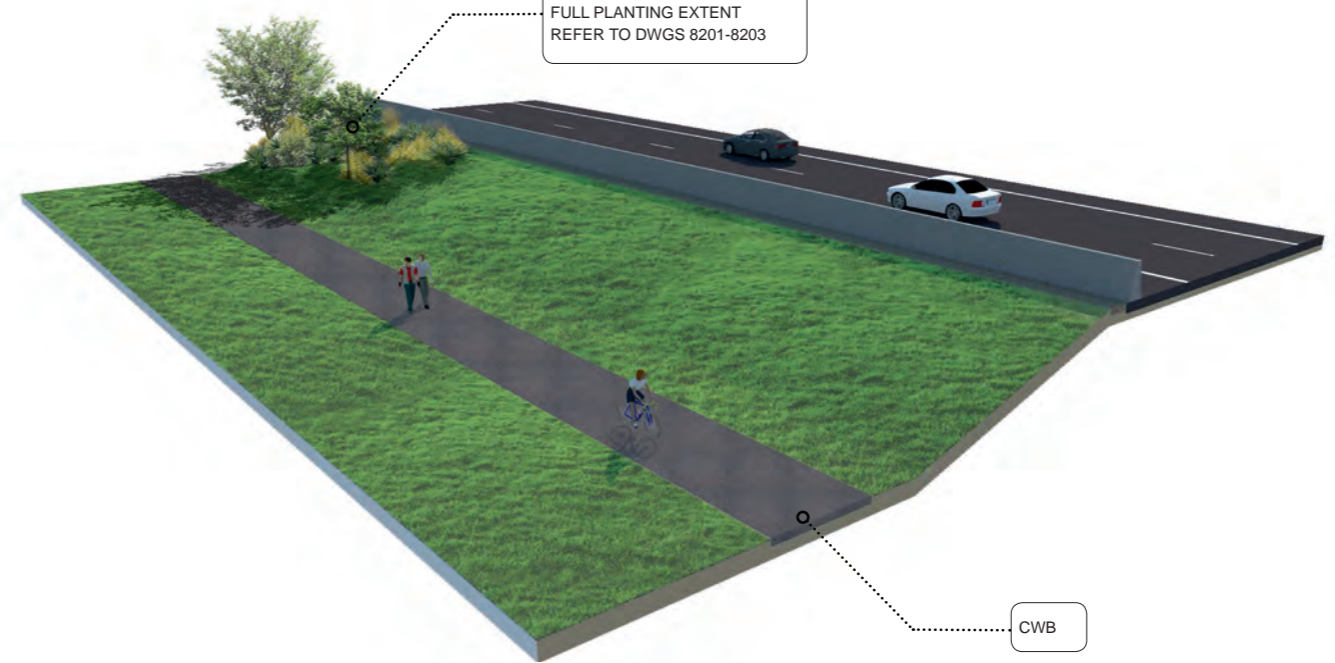


VISUALISATION EXPRESSWAY SIDE

VISUALISATION RESIDENTIAL SIDE

INDICATIVE PLANTING-FOR FULL PLANTING EXTENT REFER TO DWGS 8201-8203

INDICATIVE PLANTING-FOR FULL PLANTING EXTENT REFER TO DWGS 8201-8203



A1 REPRODUCTION SCALE
0mm
20
40
60
80
100

A3 REPRODUCTION SCALE
0mm
10
20
30
40
50

No.	Revision	By	Chk	Chk.V	Appd	Date
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Original Scale (A1)	Design	FB	18/07/14	Approved For Construction?
AS SHOWN	Drawn	VB	18/07/14	
Ridroad Scale (A3)	Drawn			
AS SHOWN	Drawn			

Project: SH1 MACKAYS TO PEKA PEKA EXPRESSWAY
RP 1012/0.00 TO 1023/5.00

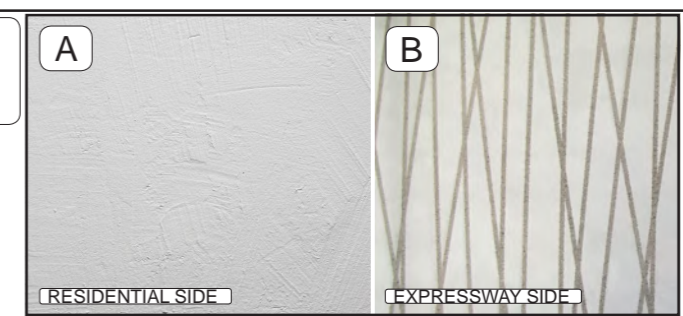
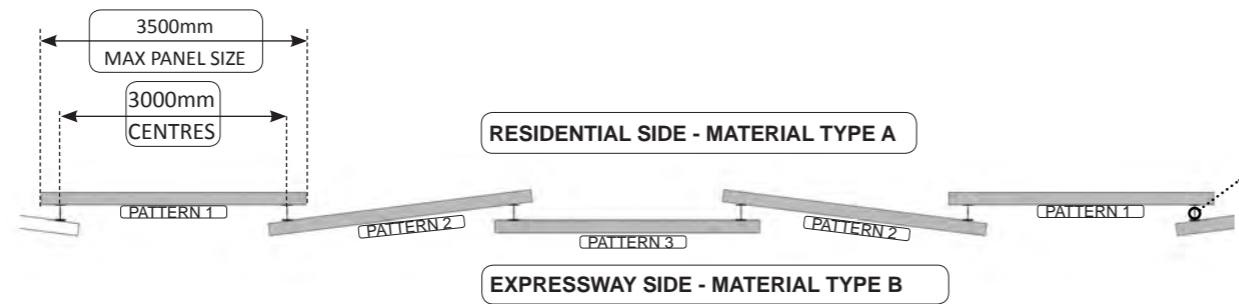
Title: SHEET 16 NOISE WALL NB1

Drawing No: M2PP-121-D-DWG-8606

Rev: B

DETAIL DESIGN (DET)

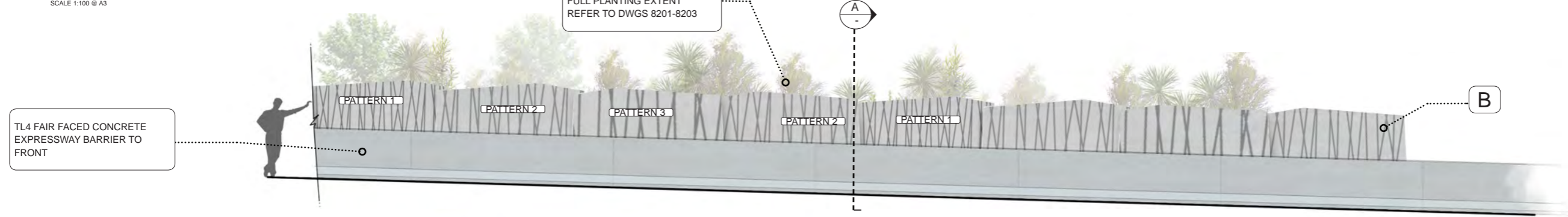
Document No.



-FAIR FACED CONCRETE WITH EXPOSED CONCRETE PATTERN.
 -CONCRETE PATTERN: VERTEX BY GRAPHIC CONCRETE
 -3 PATTERN SIZES:
 - PATTERN 1: SMALL
 - PATTERN 2: MEDIUM
 - PATTERN 3: LARGE

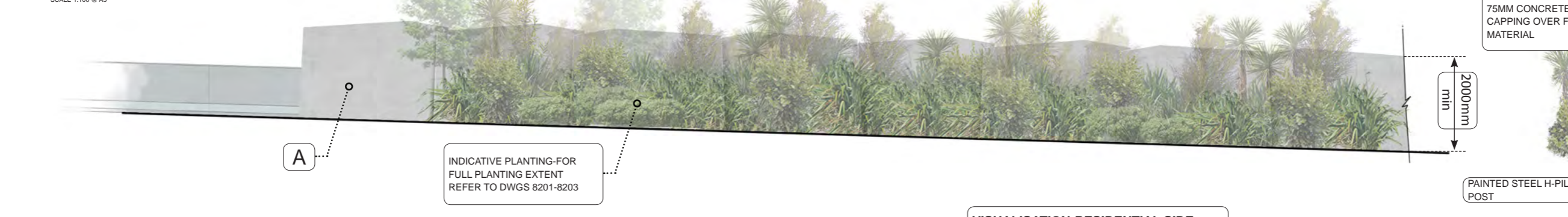
ELEVATION 1 - NOISE WALL NB2 & NB2A - EXPRESSWAY SIDE

SCALE 1:100 @ A3

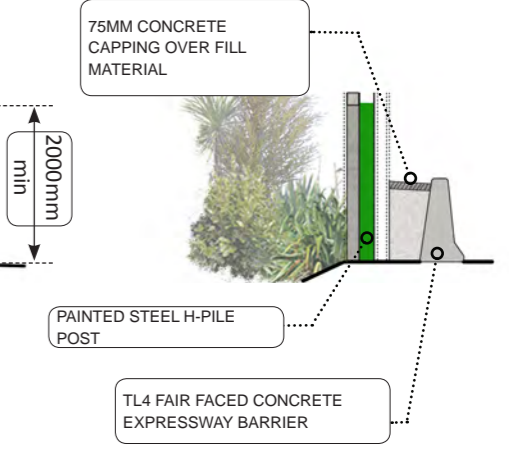


ELEVATION 2 - NOISE WALL NB2 & NB2A - RESIDENTIAL SIDE

SCALE 1:100 @ A3



SECTION A - NOISE WALL NB2 & NB2A - 2M



VISUALISATION-EXPRESSWAY SIDE

VISUALISATION-RESIDENTIAL SIDE

A1 REPRODUCTION SCALE
0mm 20 40 60 80 100

A3 REPRODUCTION SCALE
0mm 10 20 30 40 50



No.	Revision	By	Chk	Chk.V	Appd	Date
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Original Scale (A1)	Design	FB	18/07/14	Approved For Construction*
AS SHOWN	Drawn	VB	18/07/14	Date
Roadwork Scale (A3)	Design Verifier			
AS SHOWN	Design Check			

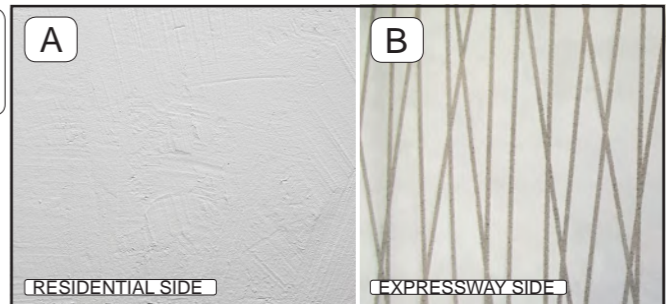
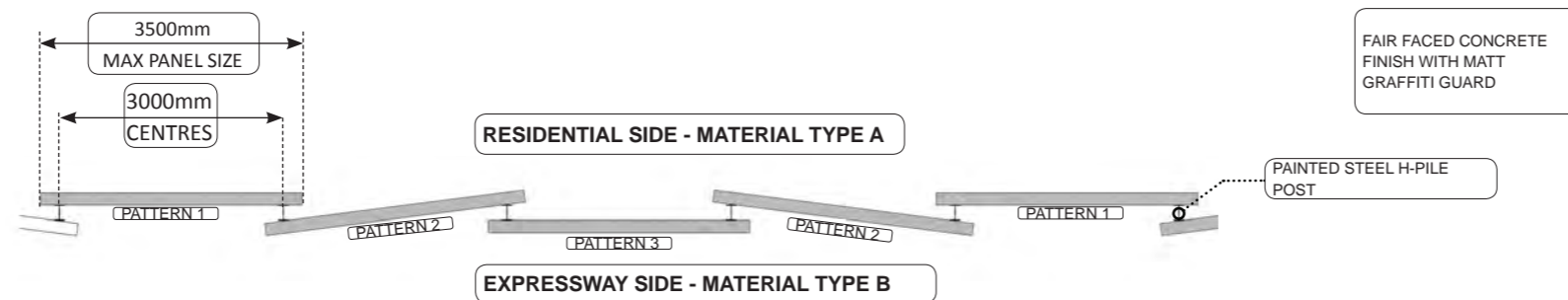
* Refer to Revision 1 for Original Signature

Project: SH1 MACKAYS TO PEKA PEKA EXPRESSWAY
 RP 1012/0.00 TO 1023/5.00

Title: SHEET 17
 NOISE WALL NB2 - 2M

Drawing No: M2PP-121-D-DWG-8607
 Rev: C

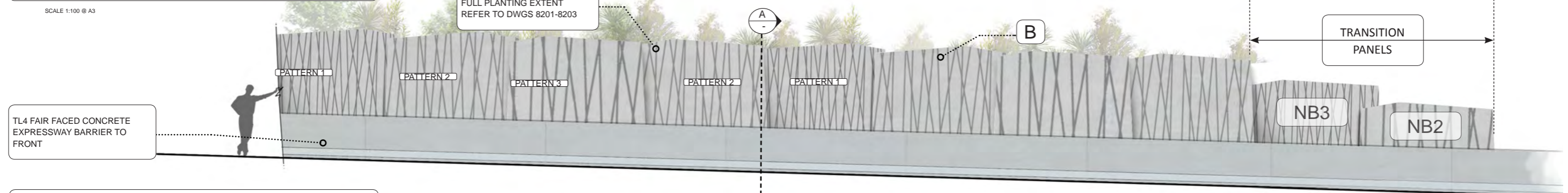
DETAIL DESIGN (DET)



- FAIR FACED CONCRETE WITH EXPOSED CONCRETE PATTERN.
- CONCRETE PATTERN: VERTEX BY GRAPHIC CONCRETE
- 3 PATTERN SIZES:
 - PATTERN 1: SMALL
 - PATTERN 2: MEDIUM
 - PATTERN 3: LARGE

ELEVATION 1 - NOISE WALL NB4 - EXPRESSWAY SIDE

SCALE 1:100 @ A3



ELEVATION 2 - NOISE WALL NB4 - RESIDENTIAL SIDE

SCALE 1:100 @ A3



VISUALISATION-RESIDENTIAL SIDE



VISUALISATION-EXPRESSWAY SIDE



A1 REPRODUCTION SCALE

A3 REPRODUCTION SCALE

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Original Scale (A1)	Design	FB	18/07/14	Approved For Construction?
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Ridroad Scale (A3)	Design Check			
AS SHOWN	* Refer to Revision 1 for Original Signature			

NZ TRANSPORT AGENCY
WAKA KOTAHU

MacKays to Peka Peka
Wellington Northern Corridor

Project: SH1 MACKAYS TO PEKA PEKA EXPRESSWAY
RP 1012/0.00 TO 1023/5.00

Title: SHEET 18
NOISE WALL NB4-3M HIGH

Drawing No: M2PP-121-D-DWG-8608

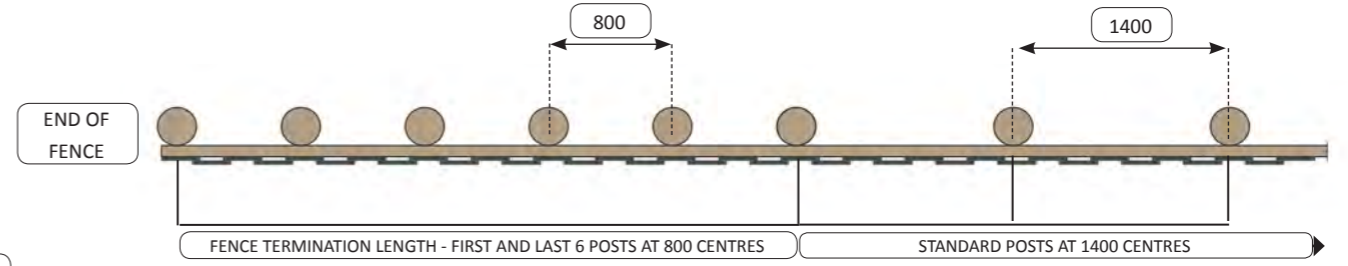
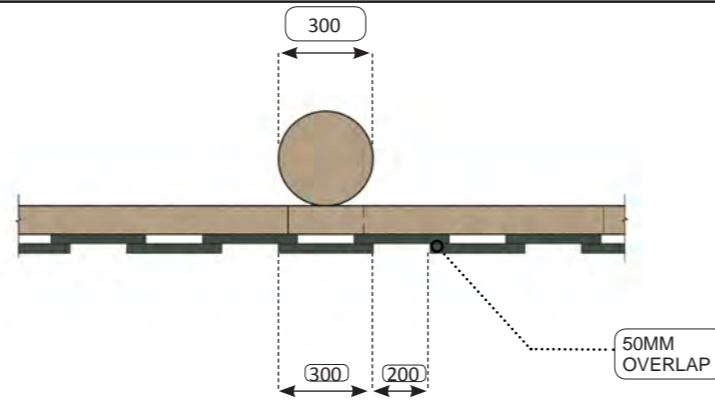
Rev: C

DETAIL DESIGN (DET)

NB7 - DRIVEN POSTS

NB7 DRIVEN POSTS:

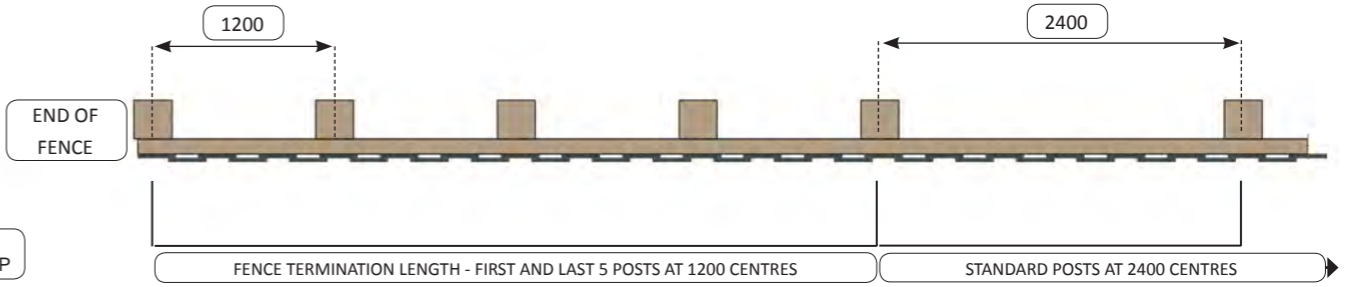
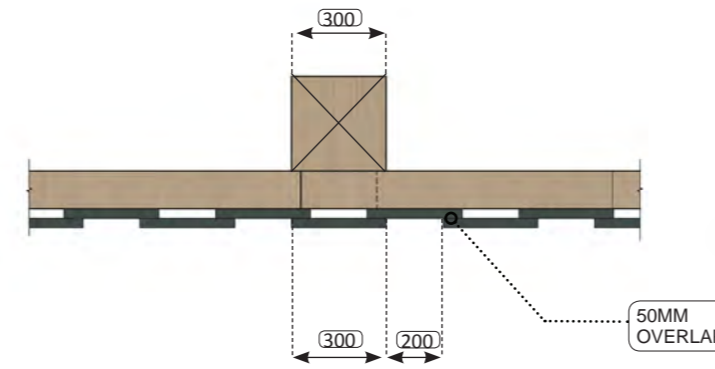
- DEPTH IN TO SAND 2000MM
- DEPTH INTO PEAT 3000MM
- POLES TO BE H5 TREATED
- FIRST AND LAST 6 POSTS FROM END OF FENCE TO BE AT 800MM CENTRES
- ALL POSTS IN BETWEEN TO BE AT 1400 CENTRES
- PALINGS 100MM IN TO GROUND



NB7 - BORED POSTS

NB7 BORED POSTS:

- DEPTH IN TO SAND 2000MM
- DEPTH INTO PEAT 3000MM
- POLES TO BE H5 TREATED
- FIRST AND LAST 5 POSTS FROM END OF FENCE TO BE AT 1200MM CENTRES
- ALL POSTS IN BETWEEN TO BE AT 2400 CENTRES
- PALINGS 100MM IN TO GROUND

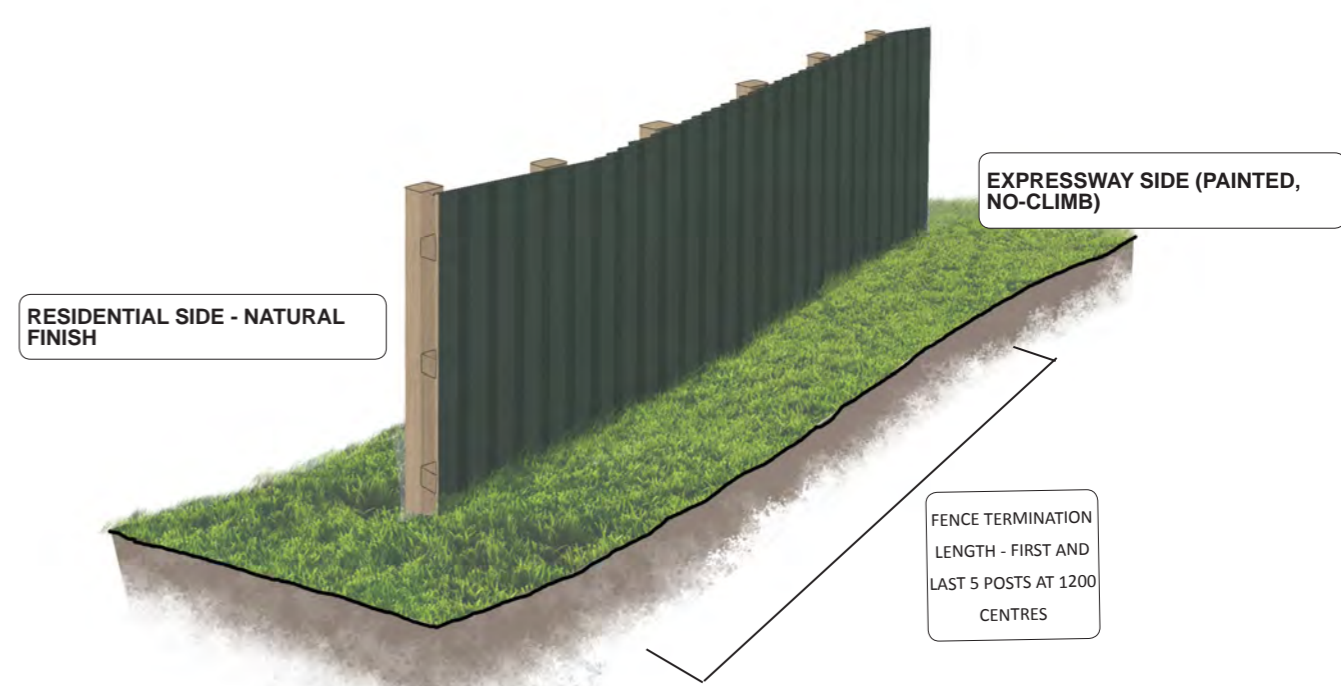


NB7 - TYPICAL ELEVATION - EXPRESSWAY SIDE

NOT TO SCALE



NB7 - TYPICAL PERSPECTIVE - EXPRESSWAY SIDE (BORED POSTS)



NB7 TIMBER NOISE FENCE:

- 2.0M HIGH
- OVERLAPPING TIMBER PALINGS
- NO-CLIMB SIDE FACING EXPRESSWAY
- NZTA APPROVED STAIN - DARK GREEN / GREY
- 30 YEAR DESIGN LIFE

COLOUR:

WOODSMAN
WATERBORNE
RESENE DATA SHEET
D57A

A1 REPRODUCTION SCALE
A3 REPRODUCTION SCALE

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AS SHOWN	Drawn	VB	18/07/14	
Reduced Scale (A3)	Design Verifier			
AS SHOWN	Design Check			Date

NZ TRANSPORT AGENCY
WAKA KOTAHI

MacKays to Peka Peka
Wellington Northern Corridor

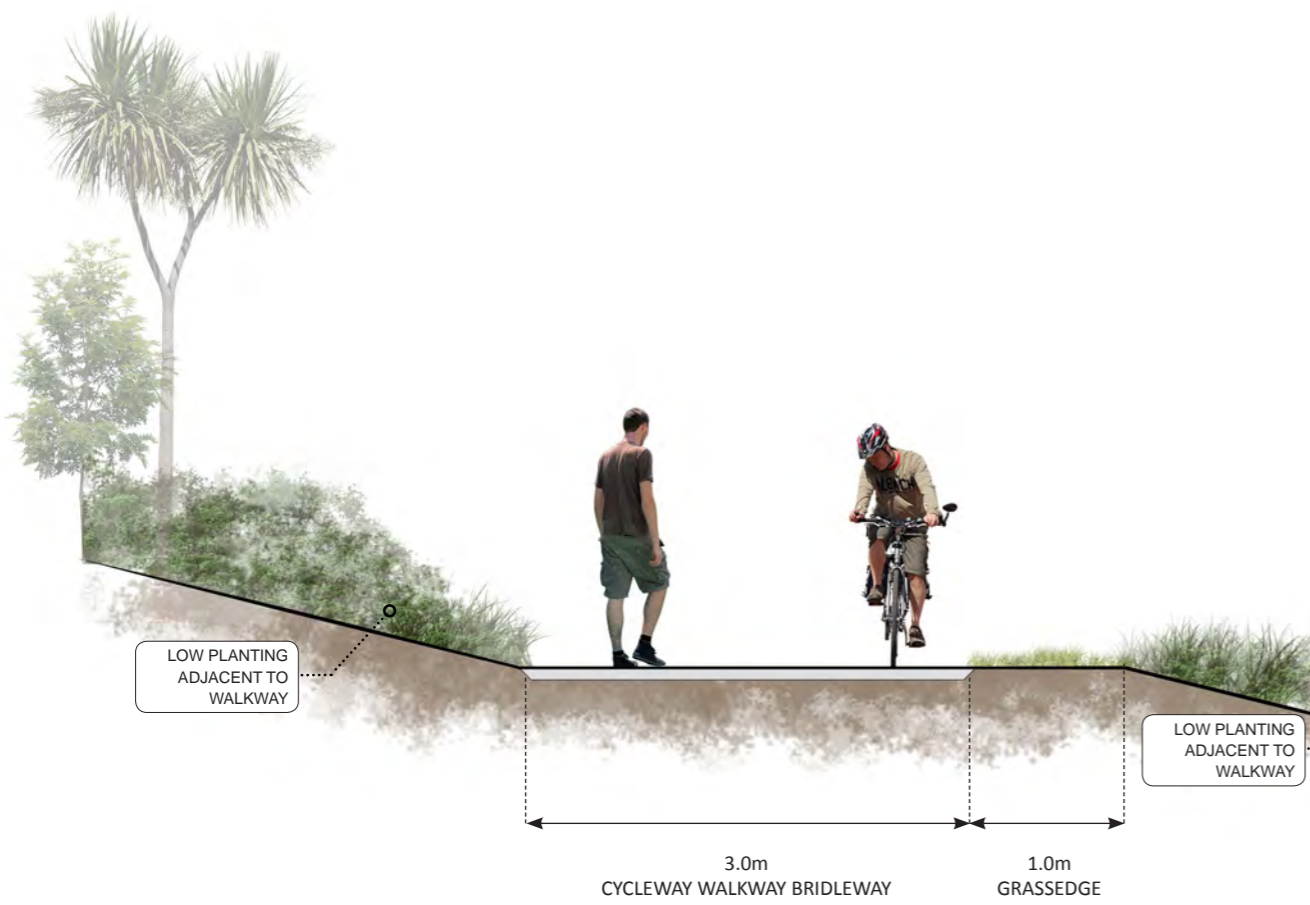
Project: SH1 MACKAYS TO PEKA PEKA EXPRESSWAY
RP 1012/0.00 TO 1023/5.00

Title: SHEET 19
NOISE WALL NB7-2.0M HIGH
TIMBER PROPERTY BOUNDARY
NOISE FENCE

Drawing No: M2PP-121-D-DWG-8609
Rev: C

CS1 - TYPICAL CYCLEWAY SECTION

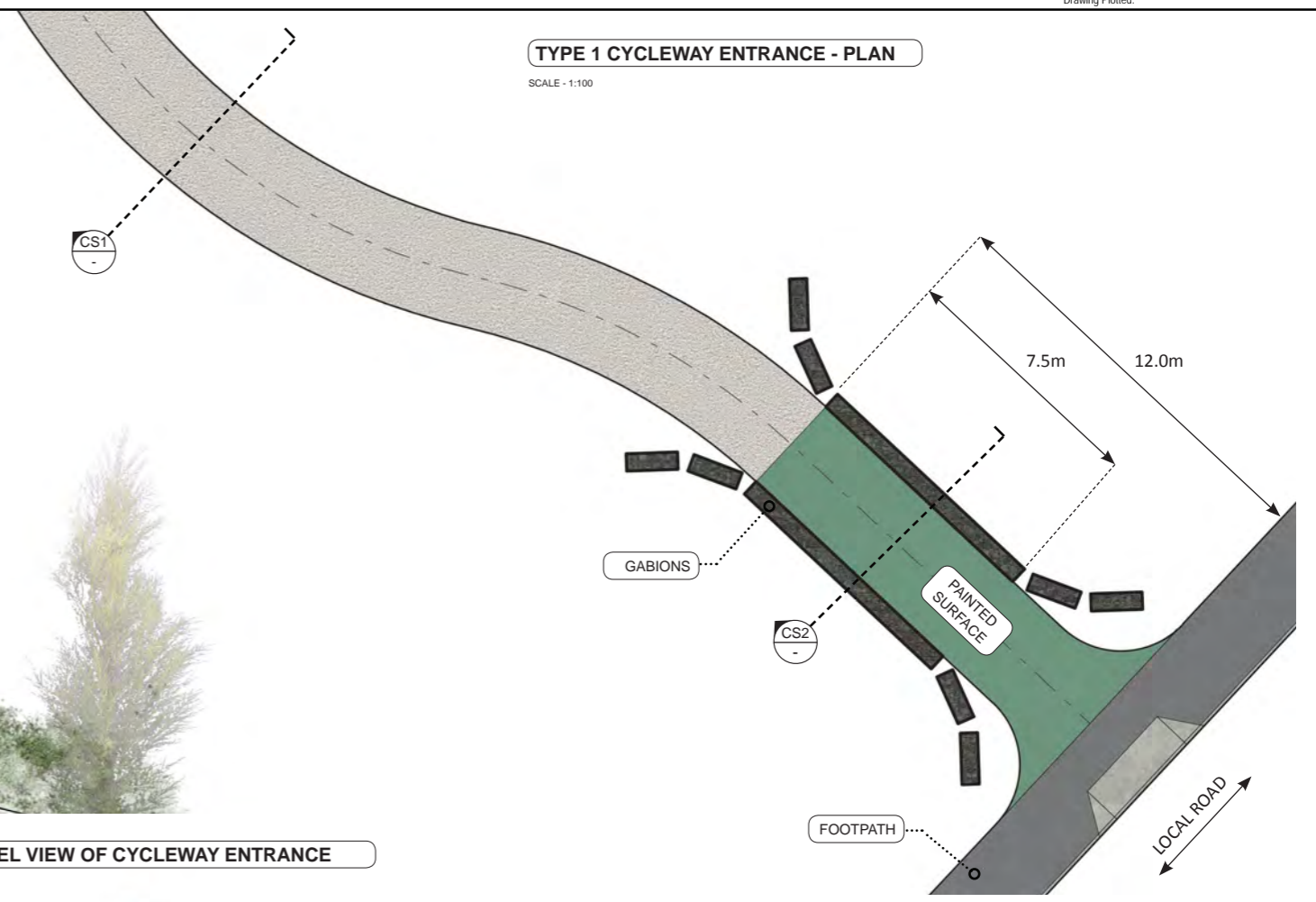
SCALE - 1:50



GROUND LEVEL VIEW OF CYCLEWAY ENTRANCE

TYPE 1 CYCLEWAY ENTRANCE - PLAN

SCALE - 1:100

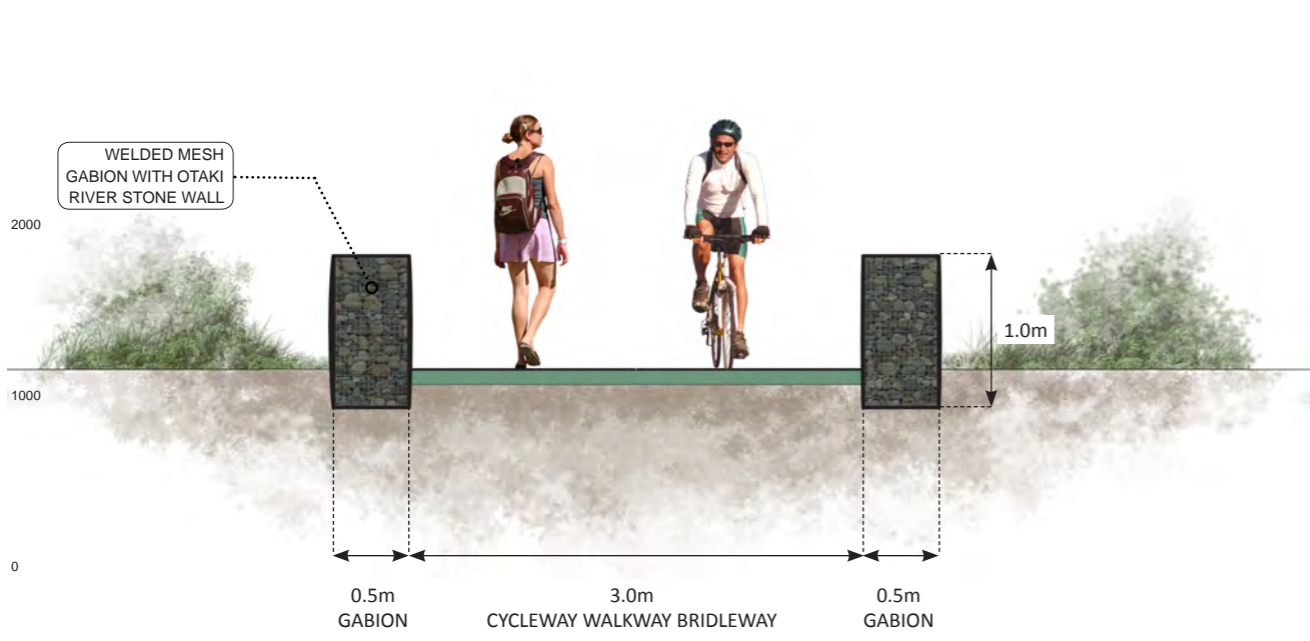


A1 REPRODUCTION SCALE

A3 REPRODUCTION SCALE

CS2 - TYPE 1 CYCLEWAY ENTRANCE

SCALE - 1:50



No.	Revision	By	Chk	Chk.V	Appd	Date
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AS SHOWN	Drawn	VB	18/07/14	Date
Ridged Scale (A3)	Design Check			
AS SHOWN	* Refer to Revision 1 for Original Signature			



Project: SH1 MACKAYS TO PEKA PEKA EXPRESSWAY
RP 1012/0.00 TO 1023/5.00

Title: SHEET 20
CWBI INTERSECTIONS

Drawing No: M2PP-121-D-DWG-8801

Rev: C

DETAIL DESIGN (DET)

PLAN OF LIGHTING LOCATIONS

- P - CWB
- D-ROAD LIGHTING
- G-ROAD LIGHTING
- U-UNDER BRIDGE LIGHTING
- Z-ROAD LIGHTING

NOTE:
INDICATIVE
LIGHTING
FROM TOC
DESIGN - POLE
HEIGHTS AND
SPACING BY
OTHERS



A1 REPRODUCTION SCALE

A3 REPRODUCTION SCALE

No.	Revision	By	Chk	Chk.V	Appd	Date
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1:1000	Drawn	VB	18/07/14	
Reduced Scale (A3)	Design	Chk	Date	
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* Refer to Revision 1 for Original Signature



Project: SH1 MACKAYS TO PEKA PEKA EXPRESSWAY
RP 1012/0.00 TO 1023/5.00

Title: SHEET 21 LIGHTING PLAN

Document No: M2PP-121-D-DWG-8701

Rev: C

DETAIL DESIGN (DET)

PLAN OF LIGHTING LOCATIONS

- P-CWB
- D-ROAD LIGHTING
- G-ROAD LIGHTING
- U-UNDER BRIDGE LIGHTING
- Z-ROAD LIGHTING

NOTE:
INDICATIVE
LIGHTING
FROM TOC
DESIGN - POLE
HEIGHTS AND
SPACING BY
OTHERS



A1 REPRODUCTION SCALE
0mm 20 40 60 80 100

A3 REPRODUCTION SCALE
0mm 10 20 30 40 50

No.	Revision	By	Chk	Chk.V	Appd	Date
C	CERTIFIED ISSUE - REV C	VB		DS		18/07/14

Original Scale (A1)	Design	FB	18/07/14	Approved For Construction*
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Reduced Scale (A3)	Dwg Verifier			
1:2000	Dwg Check			
	* Refer to Revision 1 for Original Signature			



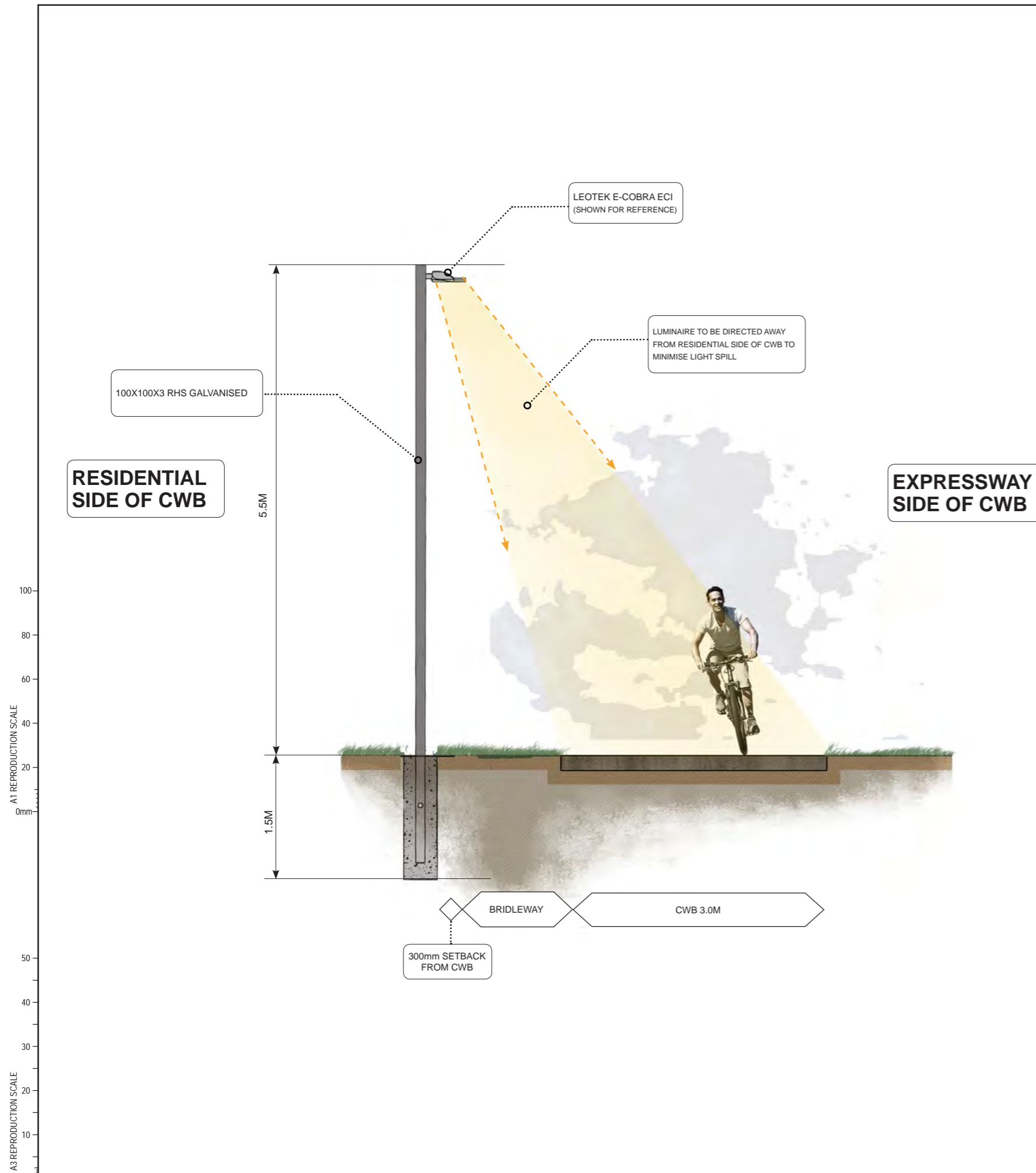
Project: SH1 MACKAYS TO PEKA PEKA EXPRESSWAY
RP 1012/0.00 TO 1023/5.00

Title: SHEET 22 LIGHTING PLAN

Drawing No: M2PP-121-D-DWG-8702

Rev: C

DETAIL DESIGN (DET)



POLE HEIGHT	POLE SPACING	EXTRAPOLATED PROJECT QUANTITY
4.5M	26M	135
5.0M	28M	126
5.5M	30M	117
6.0M	31M	114
6.5M	32M	110

OPTIMUM POLE SPACING - COLUMN HEIGHT RATIO WITH SUGGESTED LUMINR (LEOTEK E-COBRA ECI)

A1 REPRODUCTION SCALE

A3 REPRODUCTION SCALE

DETAIL DESIGN (DET)

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Reduced Scale (A3)	Design Verifier			
AS SHOWN	Design Check			

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WAKA KOTAHI

MacKays to Peka Peka
Wellington Northern Corridor

Project: SH1 MACKAYS TO PEKA PEKA EXPRESSWAY
RP 1012/0.00 TO 1023/5.00

Title: SHEET 23
INDICATIVE LIGHT POLE CONFIGURATION

Drawing No: M2PP-121-D-DWG-8703

Rev: C



A1 REPRODUCTION SCALE
0mm 20 40 60 80 100

A3 REPRODUCTION SCALE
0mm 10 20 30 40 50

DETAIL DESIGN (DET)

LIGHTING VISUALISATION - KAPITI ROAD BRIDGE CROSSING (EAST SIDE OF EXPRESSWAY LOOKING WEST)

Urban Bridges - Architectural Lighting Design Principles.

1. Uplight the undersides of the bridges from the gap between the top of spill though or vertical abutments and the bridge deck.
2. Where the bridge has columns, softly uplight the columns to accentuate the forms of the columns.
3. Stronger lighting in the more urban areas and lower/softer in the less urban.
4. White - Cool White light colours/shades to be used. Exact colours to be confirmed

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AS SHOWN			VB	18/07/14	
Reduced Scale (A3)	Design Verifier				
AS SHOWN	Design Check				

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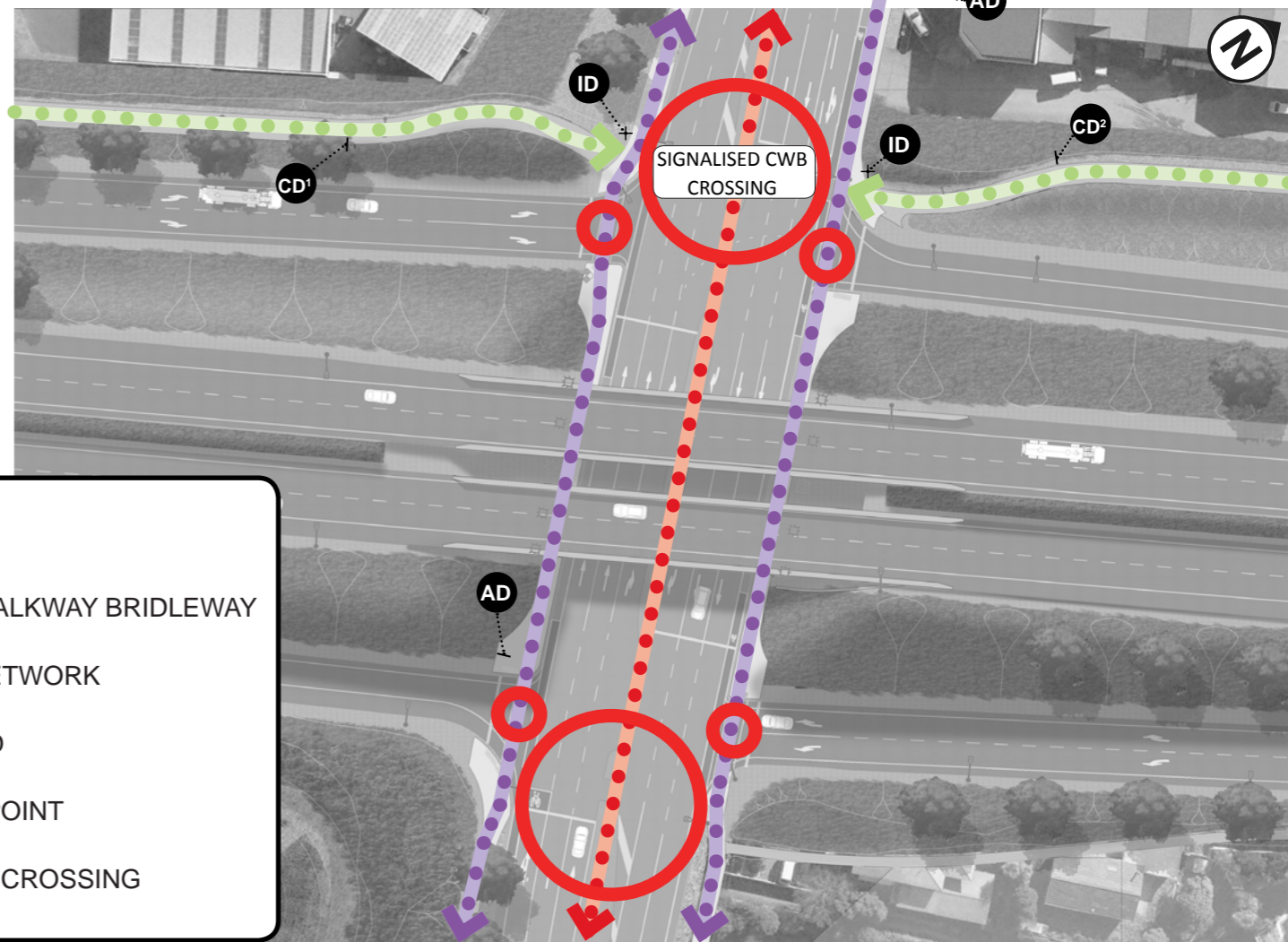
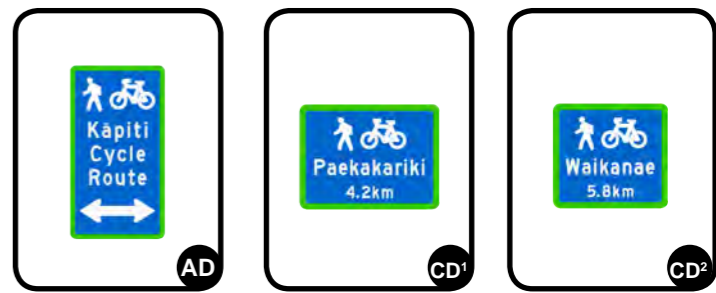
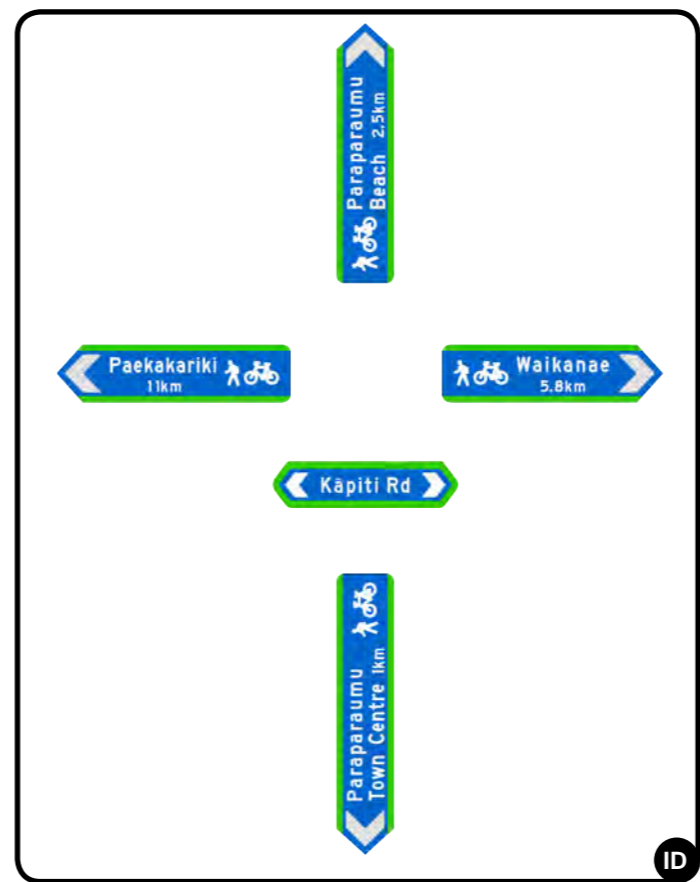
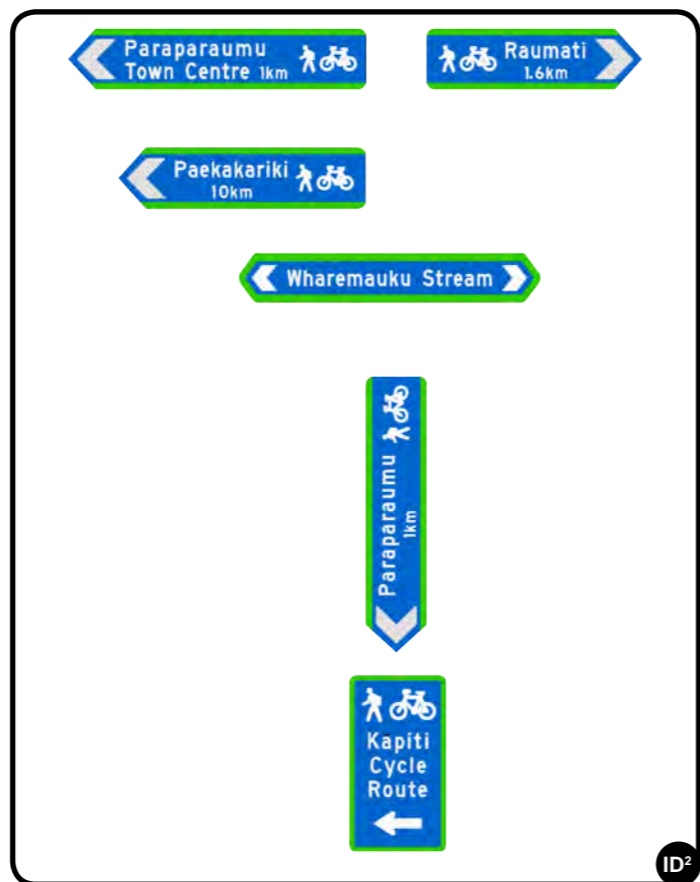
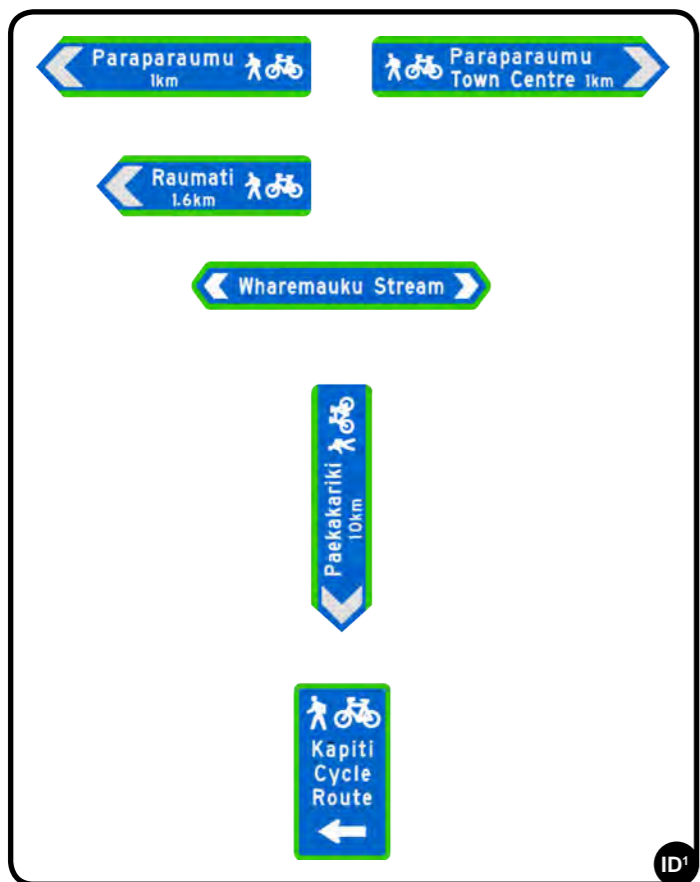
MacKays to Peka Peka

Wellington Northern Corridor

Project:	SH1 MACKAYS TO PEKA PEKA EXPRESSWAY RP 1012/0.00 TO 1023/5.00
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Title:	SHEET 24 INDICATIVE KAPITI BRIDGE LIGHTING
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Drawing No:	M2PP-121-D-DWG-8704
Rev:	C



LEGEND

- CYCLWAY WALKWAY BRIDLEWAY
- EXISTING NETWORK
- LOCAL ROAD
- CROSSING POINT
- SIGNALISED CROSSING

A1 REPRODUCTION SCALE 0mm 100

A3 REPRODUCTION SCALE 0mm 50

C	CERTIFIED ISSUE - REV C	VB	DS	18/07/14
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Original Scale (A1)	Design	FB	18/07/14	Approved For Construction
1:500	Drawn	VB	18/07/14	Date
AS SHOWN	Design Verifier			
AS SHOWN	Design Check			

NZ TRANSPORT AGENCY
WAKA KOTAHĪ

MacKays to Peka Peka
Wellington Northern Corridor

Project: SH1 MACKAYS TO PEKA PEKA EXPRESSWAY
RP 1012/0.00 TO 1023/5.00

Title: SHEET 25 SIGNAGE LOCATION PLAN

Drawing No: M2PP-121-D-DWG-8902
Rev: C

DETAIL DESIGN (DET)

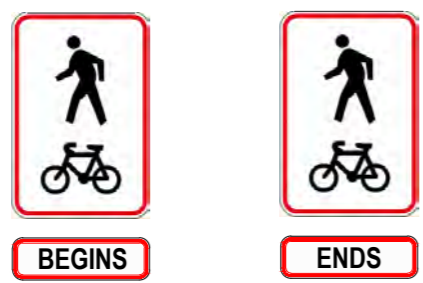
TYPICAL SIGN TYPES:

AI - ADVANCED INFO SIGNS

AT START OF ROUTE.
INCLUDES:
• MAP & INFO
• LENGTH & DURATION OF RIDE / WALK

AI - Advance Information Signs are not an essential requirement for public access tracks or cycle routes, nor are they standardised in terms of their design and layout. These signs may, if desired and appropriate, be installed at or near the start point of the route to provide detailed information, such as a map and information about the length and duration to ride etc. These signs should be clearly visible from the road, allowing cyclists and pedestrians a safe place to stop clear of the roadway or cycleway to read the information.

BE - BEGINNING AND ENDING SIGNS



BE - Begins/Ends Signs are used to indicate the start and/or end point of a cycle route. They will include route specific information. Route Begins Signs should be installed on the left hand side of the CWB immediately beyond or adjacent to any advance information sign or at a logical starting point for the cycle route.

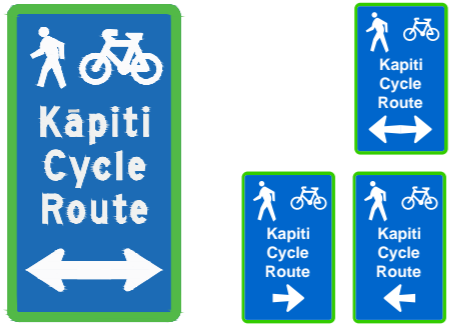
ID - INTERSECTION DIRECTION



ID - The Intersection Direction Sign is located at or as near as possible to the actual intersection. Should include both Information about the destination and the distance.

Multiple signs and destinations to be on one post

AD01 - ADVANCED DIRECTION SIGN - ON LOCAL ROAD APPROACHING CWB



AD - The purpose of the Advance Direction Sign is to give cyclists prior warning, to enable them to make decisions and, if necessary, place themselves in the best position to make any change in direction required before they reach the intersection. These signs should be used in any situation where the cyclist could easily miss making a required turn at an approaching intersection.

To occur 40-60m in advance of an intersection and should only include Information about the destination, not the distance.

CD - CONFIRMATION DIRECTION



CD - The Confirmation Direction Sign is used to confirm the direction/destination of travel after an intersection it is intended to provide assurance to cyclists. The CD sign features a straight ahead arrow and should include both Information about the destination and the distance.

As a general rule of thumb, these signs should be installed; between 20-50m beyond an intersection where an Advance Direction Sign has been used and should generally be visible from that intersection;

Cyclists should see a CD sign at least every 15-30 minutes of typical cyclist travel, or every 5-10 km.

AD - ADVANCED DIRECTION - ON CWB



AD - The purpose of the Advance Direction Sign is to give cyclists prior warning, to enable them to make decisions and, if necessary, place themselves in the best position to make any change in direction required before they reach the intersection. These signs should be used in any situation where the cyclist could easily miss making a required turn at an approaching intersection.

To occur 40-60m in advance of an intersection and should only include Information about the destination, not the distance.

LOCAL ROAD INTERSECTION SIGNS



LR + GW - Local road (LR) and Giveaway (GW) signs should to be used where the CWB crosses a local road. These are to be located at or as near as possible to the actual intersection. Where possible the LR should be kept to one per intersection and be able to be read by people on either side of the intersection. Both the LR and GW should share the same post and or be incorporated onto an existing post.

A1 REPRODUCTION SCALE
0mm
20
40
60
80
100
A3 REPRODUCTION SCALE
0mm
10
20
30
40
50

No.	Revision	By	Chk	Chk.V	Appd	Date
C	CERTIFIED ISSUE - REV C	VB		DS		18/07/14

Original Scale (A1)	Design	FB	18/07/14	Approved For Construction?
AS SHOWN	Drawn	VB	18/07/14	Date
Scale (A3)	Design Verifier			
AS SHOWN	Design Check			

* Refer to Revision 1 for Original Signature

Project	SH1 MACKAYS TO PEKA PEKA EXPRESSWAY RP 1012/0.00 TO 1023/5.00
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Title	SHEET 26 CWB SIGN TYPE SUMMARY
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Drawing No.	M2PP-121-D-DWG-8901
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Rev.	C
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